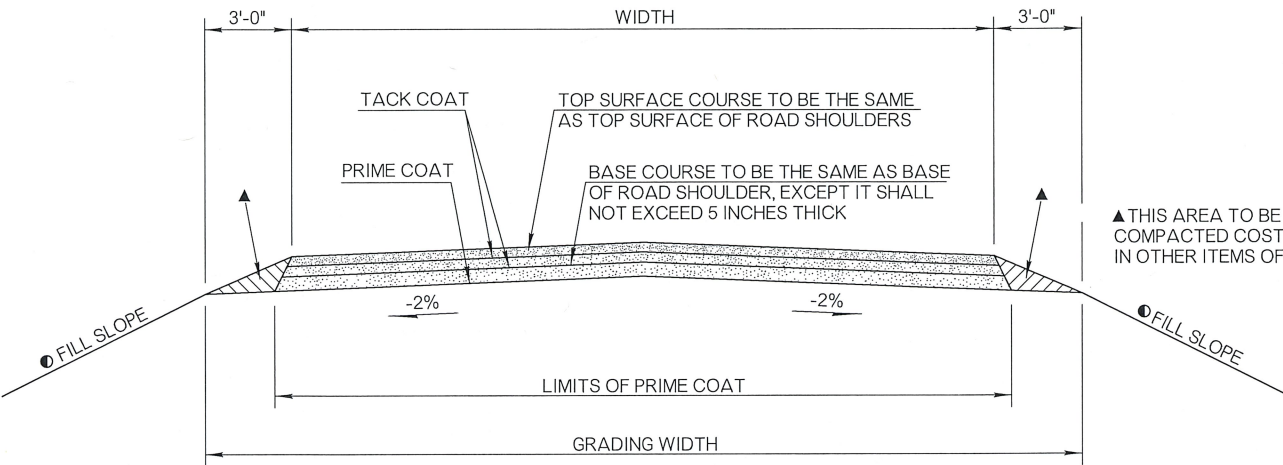
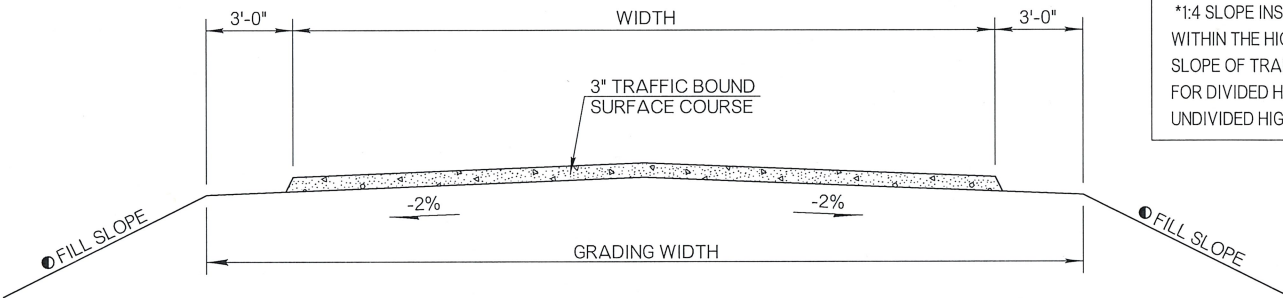


OKLAHOMA DEPARTMENT OF TRANSPORTATION		
STANDARD REVISIONS		
DESCRIPTION	DATE	

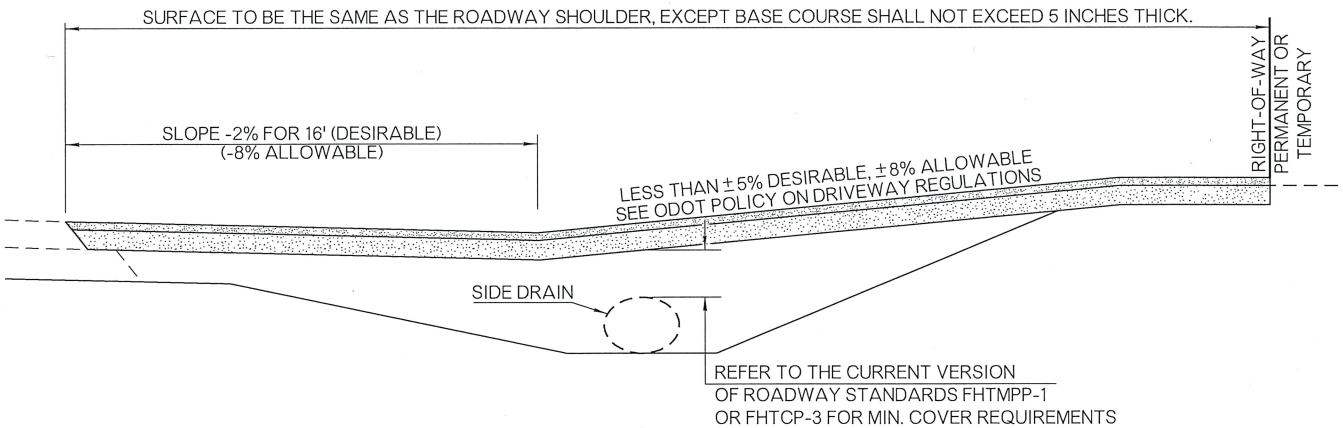


TYPICAL SECTION OF ASPHALT RETURN/DRIVE

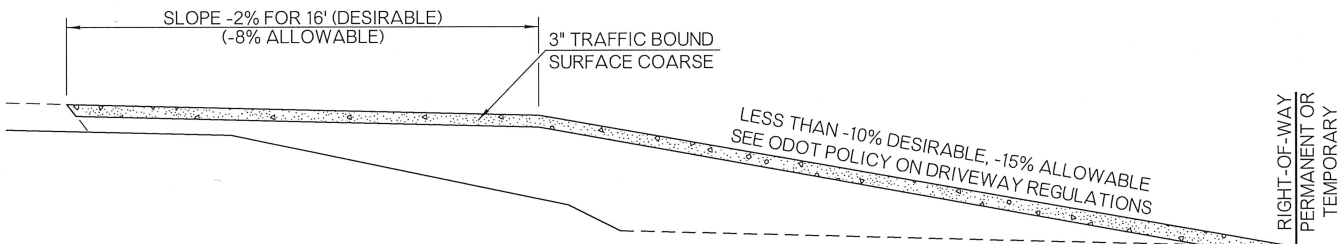


TYPICAL SECTION OF T.B.S.C. RETURN/DRIVE

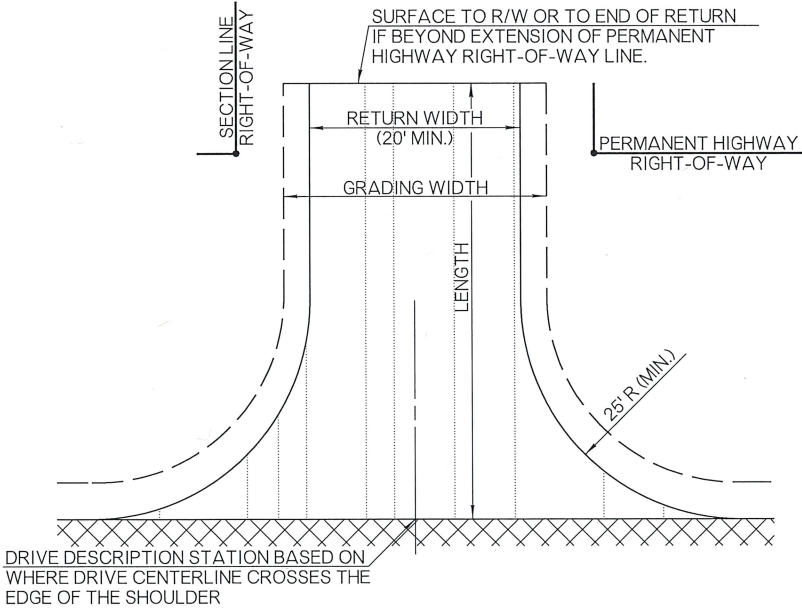
● FILL SLOPE NOTES:
 FILL SLOPE AS SHOWN IN TYPICAL SECTIONS SHALL NOT EXCEED:
 *1:3 SLOPE OUTSIDE HIGHWAY CLEARZONE
 *1:4 SLOPE INSIDE HIGHWAY CLEARZONE
 WITHIN THE HIGHWAY CLEARZONE, ADJUST SLOPE OF TRAFFIC APPROACH END TO 1:10 FOR DIVIDED HIGHWAYS AND 1:6 FOR UNDIVIDED HIGHWAYS.



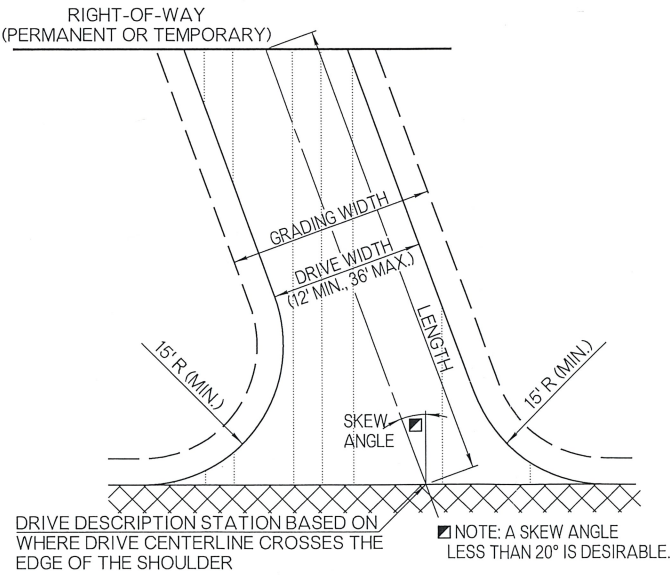
PROFILE OF TYPICAL ASPHALT RETURN/DRIVE ON ROADWAY CUT SECTION



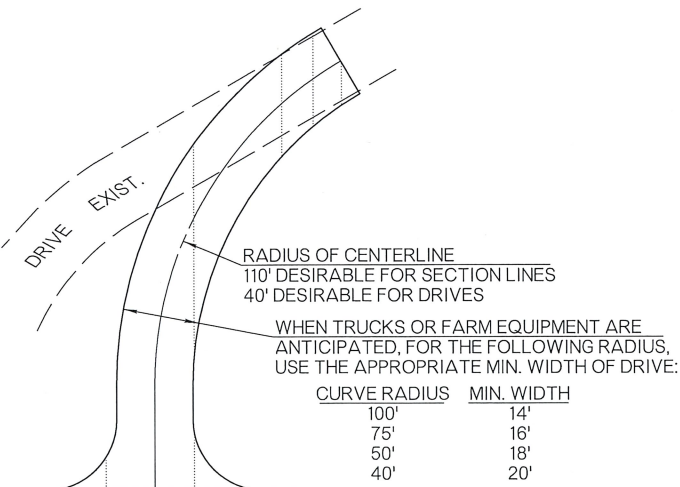
PROFILE OF TYPICAL T.B.S.C. RETURN/DRIVE ON ROADWAY FILL SECTION



PLAN TYPICAL SECTION LINE RETURN



PLAN TYPICAL DRIVE ON SKEW



SECTION LINE OR DRIVE WITH CURVED ALIGNMENT

PROPOSED DRIVES AND RETURNS SHALL MATCH EXISTING EXCEPT WHEN SKEW ANGLE EXCEEDS 20 DEGREES; IT IS THEN DESIRED TO SHIFT THE LOCATION AND CONSTRUCT USING CURVED ALIGNMENT

USEFUL ABBREVIATIONS FOR PLAN SHEETS:

ASPH.	ASPHALT
T.B.S.C.	TRAFFIC BOUND SURFACE COARSE
CONC.	CONCRETE
SEC. RET.	SECTION LINE RETURN
FIELD ENT.	FIELD ENTRANCE
PVT. DRIVE	PRIVATE DRIVE
COMM. DRIVE	COMMERCIAL DRIVE
W/S.D.	WITH SIDE DRAIN
AS DIKE	AS DIKE ACROSS DITCH

APPROVED BY

ROADWAY ENGINEER



DATE

01/23/19



ROADWAY DESIGN DIVISION STANDARD

RURAL DRIVEWAY INSTALLATION