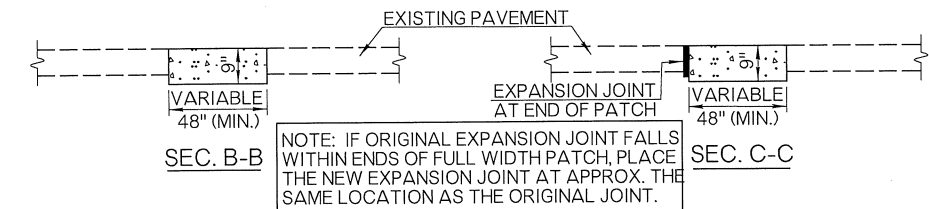
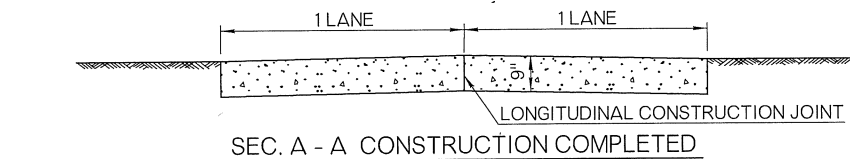
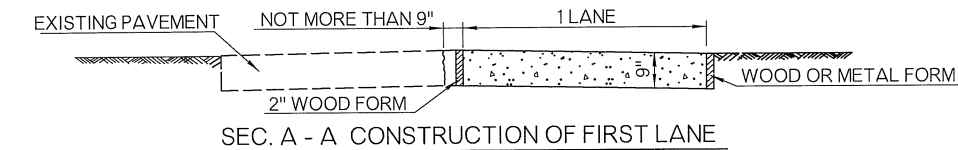
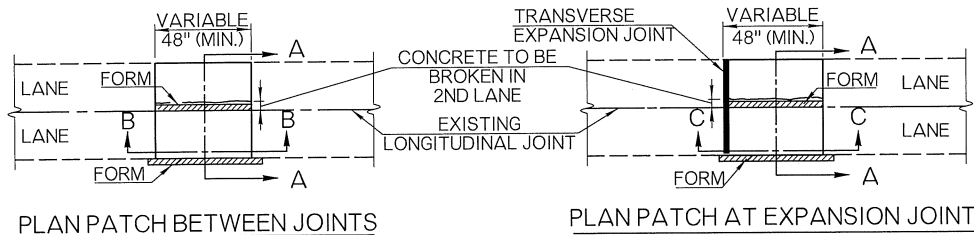
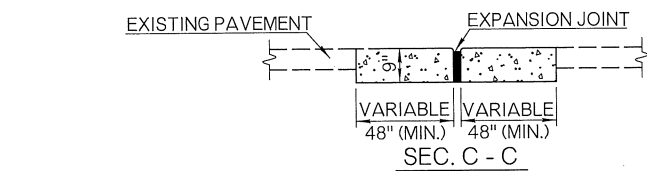
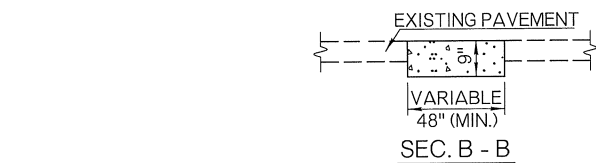
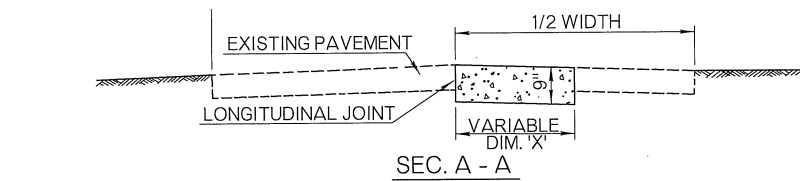
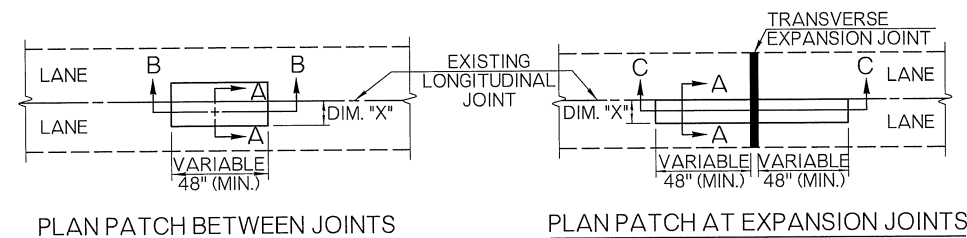


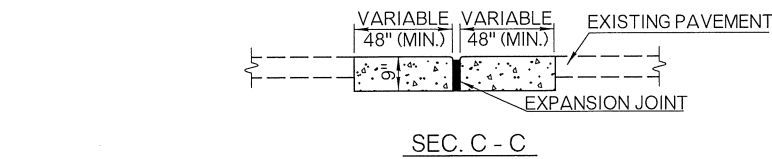
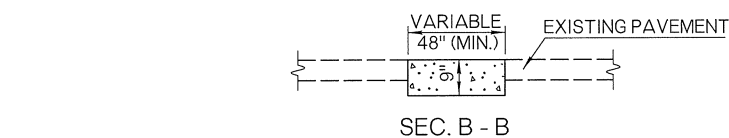
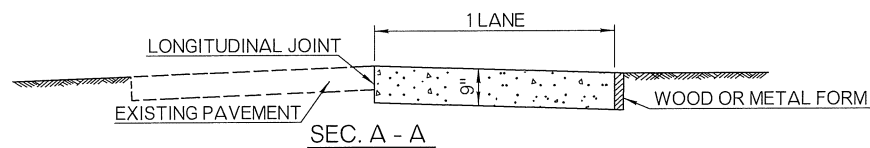
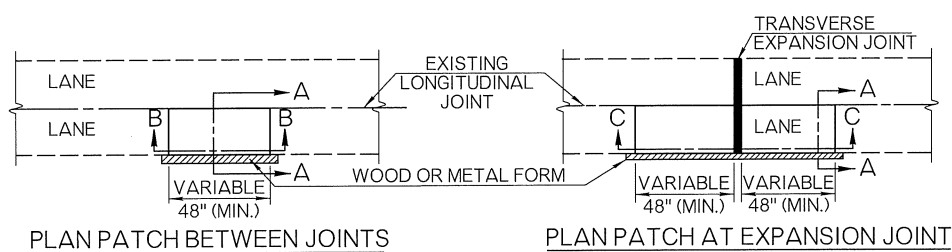
OKLAHOMA DEPARTMENT OF TRANSPORTATION		
STANDARD REVISIONS		
DESCRIPTION	DATE	



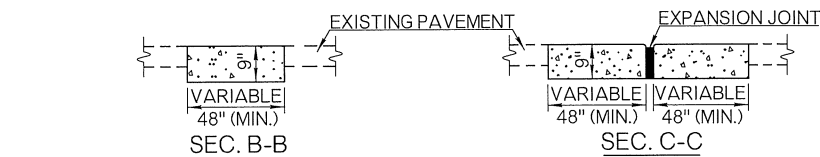
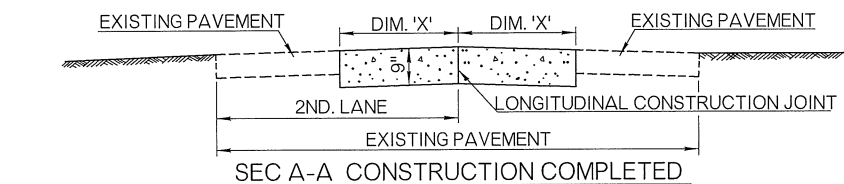
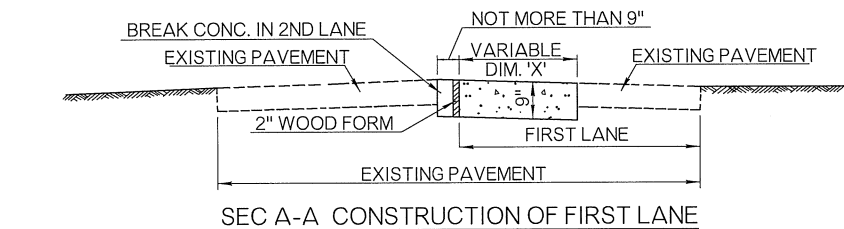
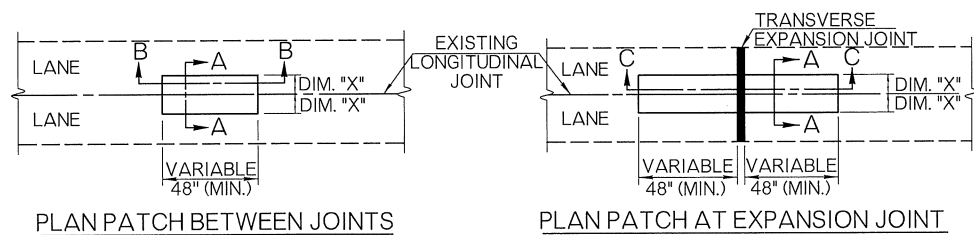
MULTI LANE PATCH (CONSTRUCTED ONE LANE AT A TIME)



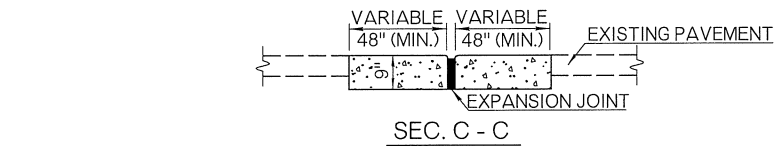
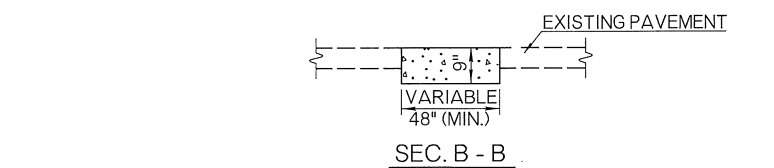
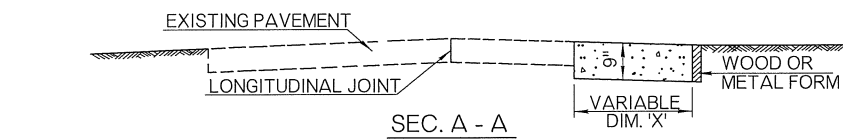
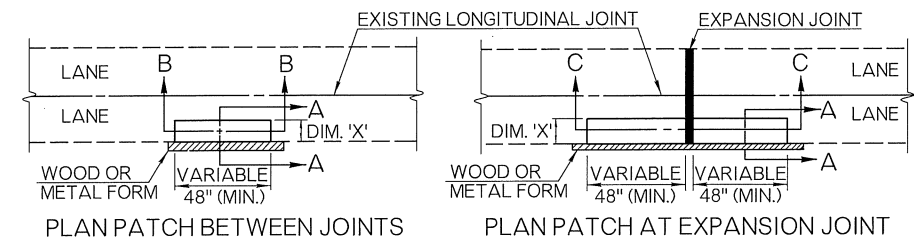
INTERIOR EDGE PATCH



SINGLE LANE PATCH



RECTANGULAR PLUG PATCH



EXTERIOR EDGE PATCH

- GENERAL NOTES
1. CROWN OF PATCH TO MATCH CROWN OF EXISTING PAVEMENT.
 2. EDGES OF PAVEMENT SLAB, OPEN JOINTS AND CONSTRUCTION JOINTS SHALL BE EDGED WITH AN EDGER HAVING A 1/2" RADIUS. JOINT ALONG A BROKEN EDGE OF THE EXISTING PAVEMENT SHALL NOT BE EDGED.
 3. CONCRETE FOR PATCHES SHALL BE HIGH-EARLY-STRENGTH CONCRETE PAVEMENT MADE WITH THE USE OF HIGH-EARLY-STRENGTH PORTLAND CEMENT OR 25% ADDITIONAL STANDARD PORTLAND CEMENT. TRAFFIC SHALL NOT BE ALLOWED ON THE PATCH FOR THE FIRST 24 HOURS, OR FOR LONGER WHEN DIRECTED BY THE RESIDENT ENGINEER.
 4. DIMENSION 'X' TO BE NOT LESS THAN 4 FEET, NOR SHALL IT EXCEED 5 FEET FOR 18 FOOT PAVEMENT OR EXCEED 6 FEET FOR 20 FOOT PAVEMENT. IF EITHER OF THESE LIMITS ARE EXCEEDED USE A HALF WIDTH PATCH.
 5. PAVEMENT SECTION DEPTH TO BE SHOWN ON PLANS.
 6. IN AREAS WHERE PATCHING IS REQUIRED, UNDERCUTTING AND BACK FILLING OF SUBGRADE SHALL BE DONE IN A MANNER APPROVED BY THE ENGINEER. BACKFILL SHALL BE COMPACTED TO NOT LESS THAN 95% OF STANDARD DENSITY. COST OF UNDERCUTTING AND BACKFILLING TO BE INCLUDED IN OTHER ITEMS OF WORK.
 7. FOR DETAILS OF JOINTS AND SEALERS, SEE ROADWAY STANDARD LECS-4.

APPROVED BY

ROADWAY ENGINEER:

DATE:

04/14/15

ROADWAY DESIGN DIVISION STANDARD

PAVEMENT RECONSTRUCTION DETAILS

OKLAHOMA DEPARTMENT OF TRANSPORTATION

2009 SPECIFICATIONS

PR-3

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R-18