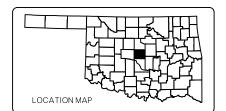
# STATE OF OKLAHOMA DEPARTMENT OF TRANSPORTATION

# ADDED COUNTY 🛕 6/15/23 ADDED STANDARD 3 6/20/23 REMOVED SHEET(S) 4

OKLAHOMA DEPARTMENT OF TRANSPORTATION



# PLAN OF PROPOSED STATE HIGHWAY

FEDERAL AID PROJECT NO. STP-252F(088)PM DISTRICT 4: DISTRICT WIDE SIGNAGE △TRAFFIC SIGNING SH-152, IH-44, & IH-240

# ANOBLE & OKLAHOMA COUNTY

CONTROL SECTION NO. 152-55-52, 44-55-05, 44-55-70, 240-55-71 STATE JOB NO. 35154 (04)

# STANDARDS TO BE INCLUDED

TRAFFIC CONTROL (2009)	TRAFFIC SIGNING (2009)			
TCS1-1-01	MSD1-1-00			
TCS2-1-00	MSD2-1-00			
TCS3-1-01	SBS2-1-00			
TCS4-1-01	SBS5-1-00			
TCS5-1-00	SBS6-1-00			HI TO TO TO THE TOTAL PARTY OF T
TCS6-1-02	GMS1-1-00			<b>[1]                                      </b>
TCS8-1-00	GMS2-1-00			Fitt - 3- 4-1
TCS9-1-01	FGS2-1-01			F T ~ ! I
TCS10-1-00	SPA1-1-00			
TCS11-1-01				1 4 1
TCS14-1-00				<i>╞</i> -┌/┖╴┰╸╷ │
TCS16-1-00				<u> </u>
TCS24-1-02				# <b> </b>
TCS25-1-00				
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<b>ROADWAY</b> (2019)				
CLB-2-2 <u></u>		<b>BEGIN PROJECT</b>		
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			END PROJECT	

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∆ T054	TRAFFIC CONTROL DETAIL SITUATION E-17
<sup>⚠</sup> T068 - T069	TRAFFIC CONTROL DETAIL SITUATION E-37B
T070 - T071	TRAFFIC CONTROL DETAIL SITUATION E-37C
↑ T072	TRAFFIC CONTROL DETAIL SITUATION E-40
<sup>∠4∆</sup> T075 - T076	TRAFFIC CONTROL DETAIL SITUATION E-45A
<u>∠4\</u> T085	TRAFFIC CONTROL DETAIL SITUATION E-67



PREPARED BY: OKLAHOMA DEPARTMENT OF TRANSPORTATION TRAFFIC ENGINEERING DIVISION KARINA BURNS, P.E.

DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION OKLAHOMA DEPARTMENT OF TRANSPORTATION DATE APPROVED DATE APPROVED STP-252F(088)PM COUNTY NOBLE & OKLAHOMA HIGHWAY SH-152, IH-44, IH-240 SHEET NO. 0001

2019 OKLAHOMA STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION GOVERN, APPROVED BY THE U.S. DEPARTMENT OF TRANSPORTATION, FEDERAL HIGHWAY ADMINISTRATION, DECEMBER 18, 2019.

# **ENVIRONMENTAL MITIGATION NOTES**

### EARTHWORK NOTE:

THE CONTRACTOR MUST ENSURE THAT ANY MATERIAL INCORPORATED INTO THE PROJECT IS FREE OF ANY HAZARDOUS, INDUSTRIAL OR CONTAMINATED WASTE, REFER TO SUB-SECTIONS 106.01 AND 202.02 OF THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

IMPORTED MATERIAL (EG. BORROW) - IF MATERIAL IS IMPORTED TO THE PROJECT AND AT ANY POINT THE MATERIAL IS DETERMINED BY THE ENGINEER TO INCLUDE ANY TYPE OF UNACCEPTABLE CONTAMINATION, THE MATERIAL MAY REQUIRE REMOVAL, IN WHOLE, OR IN PART. IF REMOVAL IS REQUIRED, THEN THE INITIAL PLACEMENT, REMOVAL AND PROPER DISPOSAL OF THIS MATERIAL SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. THE DISPOSAL OF THE UNACCEPTABLE MATERIAL SHALL BE APPROVED BY THE ENGINEER, REFER TO SUB-SECTION 107.15 OF THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

TO ASSIST THE CONTRACTOR, THE "OFF PROJECT FACILITY/BORROW SITE HAZARDOUS MATERIALS QUESTIONNAIRE" IS PROVIDED ON THE DEPARTMENT'S WEB SITE:

> https://oklahoma.gov/content/dam/ok/en/odot/documents/ok-gov-docs/programs-andprojects/environmental/hazard-questionnaire-2016.pdf

THIS QUESTIONNAIRE IS PROVIDED FOR THE CONVENIENCE OF THE CONTRACTOR SO THAT A CLEARER UNDERSTANDING OF THE CHARACTERISTICS OF THE PROPOSED SITE/ MATERIAL IS ACHIEVED. COMPLETION AND SUBMITTAL OF THIS FORM TO THE ENGINEER DOES NOT EXCUSE THE CONTRACTOR FROM PROVIDING MATERIALS THAT ARE FREE OF HAZARDOUS AND INDUSTRIAL COMPOSITION IN ACCORDANCE WITH SUB-SECTIONS 106.01 AND 202.02 OF THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

### NON-COMPLIANCE NOTE:

FAILURE TO IMPLEMENT THE COMMITMENTS SPECIFIED IN THE PLAN NOTES CAN RESULT IN NON-COMPLIANCE ISSUES ON THE PROJECT. WORK ACTIVITIES MAY BE SUSPENDED ON THE PROJECT, FOR AN UNDETERMINED DURATION, WHILE WORKING WITH REGULATORS TO BRING THE PROJECT BACK INTO COMPLIANCE. THE CONTRACTOR WILL NOT BE COMPENSATED FOR TIME LOST.

### WATER QUALITY CONVSERVATION NOTE:

APPROPRIATE BEST MANAGEMENT PRACTICES TO MINIMIZE IMPACTS FROM STORM WATER DISCHARGES AND SEDIMENTATION IN STREAMS, AS ESTABLISHED BY THE OKLAHOMA DEPARTMENT OF ENVIRONMENTAL QUALITY, SHALL BE CONSCIENTIOUSLY IMPLEMENTED THROUGHOUT THE PROPOSED CONSTRUCTION PERIODS, IN ORDER TO MINIMIZE ANY POTENTIAL IMPACTS TO ANY LISTED SPECIES. THE EFFECTIVENESS OF EROSION CONTROLS SHALL BE MAINTAINED FOR THE DURATION OF CONSTRUCTION ACTIVITIES. HAZARDOUS MATERIALS, CHEMICALS, FUELS, LUBRICATING OILS, AND OTHER SUCH SUBSTANCES SHALL BE STORED AT LEAST 100 FEET FROM THE ORDINARY HIGH WATER MARK (OHWM). REFUELING OF CONSTRUCTION EQUIPMENT SHALL ALSO BE CONDUCTED AT LEAST 100 FEET FROM THE OHWMS. SEDIMENT AND EROSION CONTROLS SHALL BE INSTALLED AROUND STAGING AREAS TO PROHIBIT DISCHARGE OF MATERIALS FROM THESE SITES, CONSTRUCTION WASTE MATERIALS AND DEBRIS SHALL BE STOCKPILED AT LEAST 25 FEET OUTSIDE OF THE OHWMS. AND THESE MATERIALS SHALL BE REMOVED AND DISPOSED OF PROPERLY FOLLOWING COMPLETION OF THE PROJECT. PREVENTATIVE MEASURES MUST BE TAKEN TO PROHIBIT THE DISCHARGE OF CONTAMINANTS INTO ANY SURFACE WATERS.

	REVISIONS	
REV. NO.	DESCRIPTION	DATE

**ENVIRONMENTAL NOTES** 

APPROVED ENVIRONMENTAL DIVISION

STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECENO. 35154(04)

REVIEW

### TRAFFIC GENERAL CONSTRUCTION NOTES

IN ACCORDANCE WITH THE OKLAHOMA UNDERGROUND FACILITIES DAMAGE PREVENTION ACT THE CONTRACTOR SHALL NOTIFY THE OKLAHOMA ONE-CALL SYSTEM, INC. 48 HOURS PRIOR TO OKLAHOMA ONE-CALL SYSTEM, INC. "CALL OKIE"

CONTRACTOR SHALL VERIFY LOCATIONS OF ALL UNDERGROUND UTILITIES IN THE VICINITY OF ALL PROPOSED SIGN AND LIGHT POLE LOCATIONS BEFORE DIGGING. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE HE MAY INFLICT TO THE EXISTING UNDERGROUND UTILITIES WITHIN THE PROJECT AREA AS A RESULT OF HIS DIGGING, EXCAVATING, ETC.

CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE AROUND ALL NEW SIGN STRUCTURES AND ATTENUATOR PADS IN MEDIANS AND ALONG THE ROADSIDE. COST OF ADDITIONAL GRADING AND RELATED WORK TO MAINTAIN POSITIVE DRAINAGE, IF NEEDED, IS TO BE INCLUDED IN OTHER ITEMS OF

EXISTING ROADWAY SHALL REMAIN OPEN DURING CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROPER BARRICADES, LIGHTS, AND SIGNING WITHIN THE LIMITS OF CONSTRUCTION. ALL CONSTRUCTION SIGNING WILL BE IMPLEMENTED ACCORDING TO CONSTRUCTION PLANS. CONSTRUCTION TRAFFIC CONTROL WILL BE INSTALLED IN A MANNER APPROVED BY THE ENGINEER, IN ACCORDANCE WITH CHAPTER VI OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, (CURRENT EDITION), AND COMPLIANT WITH APPLICABLE O.D.O.T. STANDARD DRAWINGS.

© O.D.O.T. AND THE CONTRACTOR TO COORDINATE WITH PROPER OWNERS BEFORE CONSTRUCTION TO MAINTAIN PROPERTY OWNER'S LAND ACCESS AT ALL TIMES.

SYMBOLS AND LEGENDS ARE DIAGRAMMATIC ONLY AND LOCATIONS SHALL BE ADJUSTED FOR EXISTING FIELD CONDITIONS, BUT NO MAJOR ALTERATIONS OR RELOCATIONS WILL BE MADE WITHOUT FIRST CONSULTING WITH THE TRAFFIC ENGINEERING DIVISION AT (405)521-2861

THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE THEY MAY INFLICT TO THE EXISTING UNDERGROUND UTILITIES WITHIN THE PROJECT AREA AS A RESULT OF THEIR DIGGING, TRENCHING, BORING, ETC. PRIOR TO DIGGING NEAR THE UTILITIES, THE CONTRACTOR SHALL CALL FOR A LIST OF ALL UNDERGROUND FACILITIES REGISTERED IN THE AREA OF CONSTRUCTION LISTED WITH THE FOLLOWING

THE "OKIE" NOTIFICATION CENTER 811 OR 1-800-522-6543 OR WWW.CALLOKIE.COM OR THE LOCAL COUNTY

DEPTH OF EXISTING UTILITIES SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION.

ODOT TO COORDINATE ANY DRIVEWAY ACCESS CHANGES WITH CITY AND PROPERTY/BUSINESS OWNERS.

ANY SIGNS AND/OR DELINEATORS WHICH ARE TO BE REMOVED DURING THIS PROJECT WILL BE STORED IN A RESET BY THE CONTRACTOR, COST OF THIS WORK TO BE INCLUDED IN OTHER ITEMS OF WORK.

THE STRUCTURAL DESIGN OF ALL POLES, MAST ARMS, HIGH-MAST POLES, AND OTHER SUPPORTS FOR SIGNS, LUMINAIRES, AND SIGNALS AS WELL AS THEIR CONNECTIONS SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS LUMINAIRES AND RAFFIC SIGNALS. THE MANUFACTURER SHALL ENSURE THE FOLLOWING ARE APPLIED

THE MINIMUM DESIGN WIND SPEED AND DESIGN LIFE AS REQUIRED IN THE AASHTO

THE CALCULATED STRESSES AND FORCES FROM THE DESIGN LOADINGS DO NOT EXCEED THOSE REQUIRED IN THE AASHTO SPECIFICATIONS:

A CATEGORY I FATIGUE IMPORTANCE FACTOR (IF) FOR ALL STRUCTURES; NO VIBRATORY MITIGATION SHALL BE ALLOWED. TRUCK-INDUCED GUSTS SHALL BE APPLIED TO ALL OVERHEAD TRAFFIC SIGNAL SUPPORTS.

ALL MEMBERS ARE AT LEAST THE MINIMUM THICKNESS AS REQUIRED IN THE AASHTO

THE ANCHOR BOLT DESIGN AND AMOUNT OF ANCHOR BOLTS TO BE USED SHALL BE AS

UNLESS SITE SPECIFIC GEOTECHNICAL DATA IS AVAILABLE, FOUNDATIONS SHALL BE DESIGNED UTILIZING THESE PARAMETERS; SHEAR STRENGTH OF COHESIVE SOIL (C) OI 500 PSF, ANGLE OF INTERNAL FRICTION (B) OF 22 DEGREES, AND EFFECTIVE UNIT WEIGHT

THE CONTRACTOR SHALL PROVIDE A PERSON TO BE ON 24 HOUR CALL AS NEEDED AS DETERMINED BY THE ENGINEER. THIS PERSON SHALL HOLD A CURRENT CERTIFICATION FROM THE AMERICAN TRAFFIC SAFETY SERVICE ASSOCIATION (ATSSA) OR THE OKLAHOMA TRAFFIC ENGINEERING ASSOCIATION (OTEA) AS A TRAFFIC CONTROL TECHNICIAN OR TRAFFIC CONTROL SUPERVISOR

ALL TEMPORARY TRAFFIC CONTROL DEVICES SHALL MEET ODOT'S "QUALITY STANDARDS FOR TEMPORARY TRAFFIC CONTROL DEVICES." CHANNELIZING DEVICES SHALL HAVE A MINIMUM HEIGHT OF 36 INCHES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE MAINTENANCE OF THE TEMPORARY TRAFFIC CONTROL DEVICES, AND SHALL BE RESPONSIBLE FOR REPAIRING OR REPLACING ANY DEVICE DURING CONSTRUCTION.

THE FOLLOWING AIRPORT/AIRFIELD IS LOCATED WITHIN 4 MILES OF THIS PROJECT. THIS ACTION MAY REQUIRE NOTIFYING THE FEDERAL AVIATION ADMINISTRATION (FAA) OF PROPOSED ⚠ CONSTRUCTION VIA FAA FORM 7460-1 PRIOR TO CONSTRUCTION: WILL ROGERS WORLD AIRPORT

## TRAFFIC SIGNING GENERAL CONSTRUCTION NOTES

REMOVED MATERIAL TO BECOME PROPERTY OF CONTRACTOR AND IT SHALL BE DISPOSED OF IN A

THIS PROJECT SHALL BE CONSTRUCTED WITHOUT CLOSING THE EXISTING ROAD TO LOCAL AND THROUGH TRAFFIC. SEE O.D.O.T. STANDARDS AND DETAIL DRAWINGS FOR MAINTENANCE OF LOCAL

ANY DAMAGE CAUSED BY THE CONTRACTOR TO ANY STRUCTURES, ROADWAY SURFACES, STRIPING, RAISED PAVEMENT MARKERS, GUARDRAIL, SLOPES, AND SIGNS SHALL BE REPAIRED AT CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE ENGINEER.

THIS PROJECT SHALL BE CONSTRUCTED WITHOUT CLOSING TRAFFIC ON CROSS STREETS. A MINIMUM OF ONE LANE IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES, SEE O.D.O.T. STANDARDS AND DETAIL DRAWINGS FOR MAINTENANCE OF LOCAL AND THROUGH TRAFFIC.

ALL REGULATORY SIGNS SHALL HAVE HIGH INTENSITY SHEETING. THE HIGH INTENSITY SHEETING SHALL MEET THE REQUIREMENTS OF ASTM D4956-(LATEST REVISION) FOR TYPE IV SHEETING

ALL WARNING SIGNS SHALL HAVE FLUORESCENT YELLOW SHEETING. THE FLUORESCENT YELLOW SHEETING SHALL MEET THE REQUIREMENTS OF ASTM D4956-(LATEST REVISION) REQUIREMENTS FOR TYPE

ALL GREEN AND BLUE SIGNS ON CONVENTIONAL HIGHWAYS SHALL HAVE HIGH INTENSITY SHEETING. THE HIGH INTENSITY SHEETING SHALL MEET THE REQUIREMENTS OF ASTM D4956-(LATEST REVISION) FOR TYPE

ALL PANEL AND OVERHEAD SIGNS SHALL HAVE TYPE IV HIGH INTENSITY BACKGROUND WITH TYPE XI LEGENDS AND BORDERS. THE TYPE IV BACKGROUND AND THE TYPE XI LEGENDS AND BORDERS SHALL MEET THE REQUIREMENTS OF ASTM D4956-(LATEST REVISION).

THE MANUFACTURER SHALL FURNISH A TYPE 'A' CERTIFICATION IN ACCORDANCE WITH ODOT STANDARD SPECIFICATIONS, LATEST EDITION, AND SUBSECTION 106.04. THE CERTIFICATION SHALL INCLUDE TEST RESULTS ON THE MATERIAL SUBMITTED FOR APPROVAL.

ALL BROKEN CONCRETE INCLUDING OLD SIGN FOOTINGS WITH STUBS, WASTE MATERIAL AND DEBRIS SHALL ALL BROKEN CONCRETE INCLUDING OLD SIGN FOOTHINGS WITH 5 TOBS, WAS TE MATERIAL AND DEBRIS SHALL BE DECOME THE PROPERTY OF THE CONTRACTOR AND SHALL BE REMOVED FROM THE LIMITS OF THE PROJECT AND DISPOSED OF IN AN AREA APPROVED BY THE ENGINEER. NO PAYMENT SHALL BE MADE FOR THE DISPOS. OF THIS MATERIAL. ANY PIPE POST OR WIDE FLANGE POST ABOVE THE OLD SIGN FOOTHINGS SHALL BE CUT AND HANDLED AS PROPERTY OF THE STATE AND SHALL BE NEATLY STACKED ON THE JOB SITE, AS DESIGNATED BY THE ENGINEER UNTIL SUCH TIME AS DIVISION PERSONNEL CAN REMOVE THE MATERIAL

Δ

NO SPLICES SHALL BE PERMITTED IN ANY PIPE OR WIDE FLANGE SIGN POSTS.

ALL ANCHOR BOLTS SHALL BE GRADE A-36 STEEL.

THE LOCATIONS OF THE SIGN PLACEMENT, AS SHOWN ON THE PLAN SHEETS, ARE APPROXIMATE. EXACT LOCATIONS SHALL BE DETERMINED BY THE CONTRACTOR SO THAT THE SIGN IS INSTALLED IN ACCORDANCE WITH DEPARTMENT STANDARDS AND THE MUTCD IN ORDER TO PROVIDE OPTIMUM VISIBILITY TO THE ONCOMING/APPROACHING MOTORIST. IF A PROPOSED LOCATION CONFLICTS WITH OTHER SIGNS, UTILITIES OR OTHER ROADWAY FEATURES. THE ENGINEER SHALL BE NOTIFIED.

POST LENGTHS SHOWN ON SIGN SUMMARY ARE APPROXIMATE, EXACT LENGTH SHALL BE DETERMINED BY FIELD SURVEY BY THE CONTRACTOR.

ALL REMOVED SIGNS, SIGN POSTS, BOLTS, MISCELLANEOUS HARDWARE, AND DELINEATORS SHALL REMAIN THE PROPERTY OF THE STATE. THE CONTRACTOR SHALL NEATLY STACK SUCH REMOVED MATERIAL AT A LOCATION ON THE JOB SITE AS DESIGNATED BY THE ENGINEER UNTIL SUCH TIME AS DIVISION PERSONNEL CAN REMOVE THE MATERIAL FROM THE JOB SITE.

ALL SIGNS SHALL BE REMOVED FROM THE POSTS IN A SALVAGEABLE MANNER FOR REUSE CARE SHALL BE TAKEN DURING REMOVAL AND TRANSPORTING TO ALLEVIATE DAMAGE OF MATERIALS. THE CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGE CAUSED DURING REMOVAL OF SIGNS AND SIGN POSTS

AFTER REMOVAL OF ANY SIGN FOOTINGS, THE HOLES SHALL BE FILLED WITH SOIL AND TAMPED AND SHAPED IN A MANNER APPROVED BY THE ENGINEER.

FOR NEW OR EXISTING GROUND MOUNTED SIGNS, MAXIMUM STUB POST PROJECTION ABOVE FOOTING/GROUND LINE SHALL BE 1-3/4" + /- 1/4". MAXIMUM FOOTING PROJECTION ABOVE GROUND LINE SHALL BE NO MORE THAN 2". SHOULD ADDITIONAL SOIL BE REQUIRED, THE ENGINEER WILL DESIGNATE AN AREATO OBTAIN ADDITIONAL SOIL. ALL ASSOCIATED COSTS SHALL BE INCLUDED IN OTHER ITEMS OF

UPRIGHT LENGTHS OF OVERHEAD SIGN STRUCTURES SHOWN ARE APPROXIMATE AND ACTUAL LENGTHS SHALL BE DETERMINED BY THE CONTRACTOR'S FIELD SURVEY.

DETAILS FOR MOUNTING SIGNS TO OVERHEAD STRUCTURES SHALL BE APPROVED BY THE ENGINEER AND SUBMITTED WITH SHOP DRAWINGS FOR STRUCTURES. NO MOUNTING HOLES SHALL BE PERMITTED IN OVERHEAD STRUCTURES UPRIGHT MEMBERS

CONTRACTOR SHALL PROVIDE THE ENGINEER A MINIMUM OF SEVEN (7) DAYS ADVANCE NOTICE FOR INSTALLATION OF OVERHEAD SIGN STRUCTURES AND OVERHEAD SIGNS.

ALL EXISTING AND NEW BREAKAWAY SIGN POSTS, PIPES AND WIDE FLANGE BEAMS SHALL HAVE SHEET METAL BOLT RETAINER PLATES AS SPECIFIED IN O.D.O.T. STD. FGS1- 1-(LATEST REVISION. REPLACEMENT COST OF MISSING OR DAMAGED BOLT RETAINER PLATES AND ALL ASSOCIATED HARDWARE AND LABOR SHALL BE INCLUDED IN OTHER ITEMS OF WORK.

WHERE EXISTING SIGNS NEED RE-ADJUSTMENT TO KEEP THE SIGN 1" ABOVE THE FUSE PLATE TO COMPLY WITH STD. FGS2-1-(LATEST REVISION), THE CONTRACTOR SHALL CUT ANY WIDE FLANGE SIGN POSTS THAT EXTEND ABOVE THE SIGN. THE CUT SURFACE SHALL BE GROUND SMOOTH AND GIVEN A HEAVY AND THOROUGH COAT OF ZINC-RICH PAINT IN A MANNER APPROVED BY ENGINEER.

CONTRACTOR SHALL SUBMIT SHOP DRAWINGS FOR ALL SPECIAL SIGNS TO ODOT TRAFFIC ENGINEERING  $_{\mathbb{A}}$ DIVISION FOR APPROVAL PRIOR TO MANUFACTURE.

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REV. NO.	DESCRIPTION	DATE
	REMOVED NOTE	6/14/23
$\square$	REVISED NOTE	6/15/23
$\Box$	ADDED NOTE	6/21/23
$\overline{\mathbb{A}}$	REVISED NOTE NUMBER	6/21/23

### TRAFFIC SIGNING PAY QUANTITY NOTES

- (TS-33) INCLUDED IN THIS PAY ITEM IS ALL HARDWARE ASSOCIATED WITH PROPERLY ANCHORING AND MOUNTING THE HIGHWAY SIGN IN ACCORDANCE WITH O.D.O.T. PLANS AND STANDARD DRAWINGS SSA1-1 AND SSP1-1-(
- (TS-34) INCLUDED IN THIS PAY ITEM IS THE REMOVAL OF ANY EXISTING SIGNS TO BE REPLACED BY NEW ASSEMBLIES AND THE REMOVAL OF ANY EXISTING SIGNS THAT WILL BE IN CONFLICT WITH THE NEW ROADWAY OR NEW
- OVERHEAD SIGN STRUCTURES AND SIGNS THAT ARE TO BE REMOVED, RESET, AND/OR RELOCATED SHALL BE CAREFULLY REMOVED BY THE CONTRACTOR AND STORED AT A SITE SELECTED BY THE ENGINEER. ANY DAMAGE TO THE STRUCTURES OR SIGNS DURING THE REMOVAL, TRANSPORTATION, STORAGE, RESETTING, AND/OR RELOCATION OF THE STRUCTURE OR SIGN SHALL BE REPAIRED BY, AND AT THE EXPENSE OF THE
- "REMOVAL OF EXISTING SIGNS" SHALL INCLUDE THE REMOVAL OF A COMPLETE SIGN ASSEMBLY WHICH MAY INCLUDE MULTIPLE SIGNS, POSTS, FOOTINGS, AND ANY FOOTINGS ADJACENT TO THE SIGN ASSEMBLY. WHEN APPROVED BY THE ENGINEER, FOOTINGS MAY BE OBLITERATED TO A POINT BELOW GROUND LEVEL IN LIEU OF BEING COMPLETELY REMOVED. SEE GENERAL CONSTRUCTION NOTES FOR DISPOSAL OF OLD CONCRETE

# SPECIAL NOTES

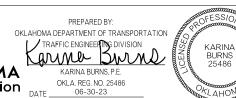
- (SP-1) QUANTITY TO INCLUDE REMOVAL OF SIGNS MOUNTED TO STRUCTURES AS SPECIFIED IN THE SIGN SUMMARY A
- (SP-2) QUANTITY TO INCLUDE REMOVAL AND REPLACEMENT OF OVERHEAD SIGN PANELS AS SPECIFIED IN THE SIGN SUMMARY AND PROPOSED SIGNING PLAN.
- (SP-3) QUANTITY TO INCLUDE REMOVAL OF ADVISORY SPEED PLAQUES AS SPECIFIED IN THE SIGN SUMMARY AND
- (SP-4) ATTENUATORS SHALL BE QUADGUARD ELITE. PRICE BID INCLUDES INSTALLATION OF A 6-INCH THICK REINFORCED CONCRETE PAD INCLUDING LABOR, CONCRETE AND STEEL MATERIALS, AND ANY ASSOCIATED COSTS TO PROVIDE A SUITABLE PAD FOR THE IMPACT ATTENUATOR IN ACCORDANCE WITH THE MANUFACTURER'S REQUIREMENTS.
- (SP-5) SHALL BE MASSH-400 MULTI-DIRECTIONAL SIGN SUPPORT HARDWARE SYSTEM

FOR APPROVAL.

- (SP-6) INCLUDED IN THIS ITEM SHALL BE THE COST OF THE REMOVAL OF ASSOCIATED SAND FILLED ATTENUATORS AND ATTENUATOR PADS. THE PRICE BID FOR THIS ITEM SHALL INCLUDE THE COST OF OBLITERATING THE CONCRETE PADS FOR THE EXISTING ATTENUATOR PADS BELOW GROUND LEVEL TO THE SATISFACTION OF
- (SP-7) PROPOSED OVERHEAD GUIDE SIGN STRUCTURES TO BE PLACED 25' OR APPROVED OFFSET DOWNSTREAM/BEHIND THE EXISTING STRUCTURE TO BE REPLACED UNLESS OTHERWISE SPECIFIED. THE EXISTING GUIDE SIGN STRUCTURE TO BE REMOVED WILL BECOME PROPERTY OF THE CONTRACTOR AND
- (SP-8) INCLUDED IN THIS PAY ITEM IS THE REMOVAL OF OVERHEAD SIGNS ON OVERHEAD STRUCTURES THAT ARE
- (SP-9) PAY ITEM IS TO ADDRESS THE ENGINEERING DESIGN MATERIALS FABRICATION INSTALL ATION AND ANY INCIDENTALS FOR THE BRIDGE MOUNTED SIGN SUPPORTS. EXISTING BRIDGE MOUNTED SIGN MOUNTS ARE TO BE REMOVED AND REPLACED WITH NEW. SUBMIT OK PE STAMPED CALCULATIONS AND DESIGN DRAWINGS BE REMOVED AND REPLACED WITH NEW. SUBMIT OR PESTAMPED CALCULATIONS AND DESIGN DRAWINGS TO THE ENGINEER FOR REVIEW AND ACCEPTANCE, NO MOUNTING OR INSTALLATION OF THE SIGN MOUNT STRUCTURES IS TO BE PERFORMED PRIOR ACCEPTANCE OF THE SUBMISSION. EFFECTS ON THE BRIDGE ARE TO BE CONSIDERED IN THE SUBMITTED CALCULATIONS WITH RESPECT TO THE AASHTO BRIDGE DESIGN SPECIFICATIONS, DESIGN THE AUGUST STRUCTURE IN ACCORDANCE WITH THE LATEST EDITION OF THE AASHTO SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS. SIGN MOUNTED STRUCTURES ARE TO BE GALVANIZED. SIGNS ARE TO BE MOUNTED PERPENDICULAR TO THE ROADWAY Λ

PAY ITEM TO INCLUDE THE REMOVAL OF EXISTING LIGHTS AND CATWALKS ON OVERHEAD STRUCTURES THAT WILL REMAIN WITHIN THE PROJECT LIMITS. LIGHTS AND CATWALKS TO BECOME PROPERTY OF THE CONTRACTOR. AT LOCATIONS WHERE TRAFFIC CONTROL HAS NOT BEEN SPECIFIED ELSEWHERE IN THE PLANS, CONTRACTOR SHALL SUBMIT TRAFFIC CONTROL PLAN TO ODOT TRAFFIC ENGINEERING DIVISION

DIVISION 4



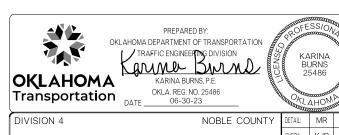
OKLAHOMA Transportation DATE\_\_\_ NOBLE COUNTY DETAIL: MR 6/23 CHECK: KJB SUMMARY OF PAY ITEMS AND NOTES ENGINEER: KJB 6/23

(SIGNING) GROUP: (SHEET 1 OF 2)

STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECENO. 35154 (04) SHEETNO. AT01

	REVISIONS	$\overline{}$
REV. NO.	DESCRIPTION	DATE
$\triangle$	REVISED QUANTITY	6/30/23
$\boxed{\mathbb{A}}$	ADDED NOTE	6/30/23
$\boxed{\mathbb{A}}$	REVISED NOTE NUMBER	6/30/23
4	ADDED PAY ITEM	6/30/23

)300 TRΔ	FEIC SIGN	PAY QUANTITY SCHEDULE			
PAY ITEM	CODE NO.	DESCRIPTION		UNIT	QUANTITY
516(A)	8240	DRILLED SHAFTS 60" DIAMETER		LF	684.00
542	9101	(PL)INSTALLATION OF BRIDGE ITEMS	(SP-9)	EA	5.00
619(B)	6416	REMOVAL OF CONCRETE MEDIAN BARRIER		LF	247.00
627(A)	6200	CONCRETE LONGITUDINAL BARRIER, DESIGN 1		LF	905.00
804(A)	2200	STRUCTURAL CONCRETE		CY	55.67
804(B)	2300	REINFORCING STEEL		LB	12,889.00
805(A)	3236	(PL)REMOVAL OF OVERHEAD SIGN STRUCTURE (TS-39), (SF	P-6, 7, 8)	EA	10.00
805(A)	3252	(PL)REMOVAL OF EXISTING SIGNS (TS-34, 41),	(SP-1,10)	EA	62.00
805(D)	3528	(PL)REMOVE & RESET EXISTING SIGNS	(TS-41)	EA	6.00
836	7100	REGULATORY OR WARNING SIGN ASSEMBLY		EA	1.00
850(A)	1200	SHEET ALUMINUM SIGNS		SF	77.50
850(B)	1310	EXTRUDED ALUMINUM PANEL SIGNS		SF	3,439.70
850(B)	1320	EXTR.ALUM.PAN.SGN.(OVRHD SIGNS)		SF	10,330.80
851(A)	2220	8"@31 GALV.STL.WD.FLANGE BM.POST	(TS-33)	LF	81.00
851(A)	2235	10"@45 GALV.STL.WD.FLANGE BM.POST	(TS-33)	LF	617.50
851(C)	2435	4? SQ TUBE SIGN SUPPORT (OMNI-DIRECT) (TS-3:	3), (SP-5)	LF	280.00
852(D)	3528	OVHD.SN.STR., MONOTUBE TYPE B 90'	(SP-7)	EΑ	1.00
852(D)	3532	OVHD.SN.STR., MONOTUBE TYPE B 95'	(SP-7)	EΔ	1.00
852(D)	3536	OVHD.SN.STR., MONOTUBE TYPE B 100'	(SP-7)	EΑ	2.00
852(D)	3540	OVHD.SN.STR., MONOTUBE TYPE B 105'	(SP-7)	EΑ	3.00
852(E)	3600	OVHD.SN.STR., MONOTUBE TYPE C	(SP-7)	EA	5.00
871(A)	2200	IMPACT ATTENUATOR	(SP-4)	EA	10.00
890	1110	(PL) TRAFFIC ITEMS	(SP-2,3)	EA	13.00



ENGINEER: KJB 6/23

SUMMARY OF PAY ITEMS AND NOTES (SIGNING) (SHEET 2 OF 2)

STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECENO. 35154 (04) SHEETINO. AT02

### TRAFFIC CONSTRUCTION PAY QUANTITY NOTES

- (TC-1) THE CONTRACTOR SHALL FURNISH AND INSTALL SUCH LIGHTS, SIGNS, BARRICADES, AND PROVIDE FLAGGERS NECESSARY FOR THE CONTROL, SAFETY, AND MAINTENANCE OF TRAFFIC WHEN INSTALLING, RELOCATING OR DELIVERING PORTABLE LONGITUDINAL BARRIER.
- (TC-21) INCLUDED IN THE COST OF THIS ITEM SHALL BE INSTALLATION, MAINTENANCE, AND REMOVAL. THIS ITEM SHALL BE RID ACCORDINGLY
- (TC-26) ALL CONSTRUCTION TRAFFIC CONTROL WILL BE IMPLEMENTED ACCORDING TO CONSTRUCTION PLANS, AND INSTALLED IN A MANNER APPROVED BY THE ENGINEER, IN ACCORDANCE WITH CHAPTER VI OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, (CURRENT EDITION), AND COMPLIANT WITH APPLICABLE O.D.O.T. STANDARD DRAWINGS. PRICE BID FOR THIS ITEM SHALL BE PAYMENT IN FULL FOR THE INSTALLATION, MAINTENANCE AND SUBSEQUENT REMOVAL OF ALL NECESSARY CONSTRUCTION TRAFFIC CONTROL DEVICES REQUIRED FOR COMPLETION OF THE PROJECT. ALL SIGNS AND BARRICADES WHICH ARE SHOWN WITH TYPE 'A' LIGHTS IN THE STANDARD DRAWINGS SHALL HAVE THE CORRESPONDING LIGHT ATTACHED DURING NON-DAYLIGHT HOURS.
- (TC-30) INCLUDED IN THIS ITEM ARE ALL S.C.S. (SPECIAL CONSTRUCTION SIGNING) SIGNS WHICH ARE BETWEEN 16.0 S.F. AND 32.99 S.F. ALSO INCLUDED IN THIS ITEM SHALL BE THE COST OF INSTALLATION, MAINTENANCE, AND REMOVAL OF THESE SIGNS.
- (TC-33) ALL CONSTRUCTION WORK ZONE SIGNS SHALL HAVE FLUORESCENT SHEETING. THE FLUORESCENT SHEETING SHALL MEET THE REQUIREMENTS OF ASTM D4956 (LATEST REVISION) THE MANUFACTURER SHALL FURNISH A TYPE 'D' CERTIFICATION IN ACCORDANCE WITH O.D.O.T. STANDARD SPECIFICATIONS (CURRENT EDITION) SUBSECTION 106.04. THE CERTIFICATION SHALL INCLUDE TEST RESULTS ON MATERIAL SUBMITTED FOR
- (TC-52) ANY USED \* TO BE PLACED ON THIS PROJECT SHALL BE SUBJECT TO INSPECTION AND APPROVAL, BY THE OKLAHOMA DEPARTMENT OF TRANSPORTATION, TO ASSURE THAT THEY ARE IN GOOD WORKING CONDITION, PRIOR TO PLACEMENT ON THE PROJECT.
  - \* TRUCK MOUNTED ATTENUATOR
    CHANGEABLE MESSAGE SIGN
    CONSTRUCTION ZONE IMPACT ATTENUATOR
    SAND FILLED IMPACT ATTENUATOR
- ANY TRUCK MOUNTED ATTENUATOR USED ON THIS PROJECT SHALL HAVE PASSED ALL MANDATORY AND OPTIONAL TESTS LISTED IN NCHRP 350, TL-3 CRITERIA. THIS ITEM IS TO BE USED WHERE SHOWN IN THE STANDARD DRAWINGS OR AT THE DISCRETION OF THE ENGINEER ON SHADOW VEHICLES PROTECTING THE WORK AREAS AND TEMPORARY ROADSIDE HAZARDS
- (TC-77) TRUCK MOUNTED ATTENUATORS ARE TO BE INSTALLED ON NON-STATE OWNED TRUCKS HAVING A MINIMUM GROSS WEIGHT RATING OF 15,000 POUNDS. EACH OF THESE TRUCKS SHALL ALSO BE EQUIPPED WITH AN ARROW DISPLAY (TYPE B).
- (TC-84) 270 CONSTRUCTION CALENDAR DAYS WERE USED TO COMPUTE THE SIGN DAY PAY ITEMS. THE AMOUNT OF CALENDAR DAYS USED TO COMPUTE THE SIGN DAY PAY ITEMS IS AN ESTIMATED QUANTITY ONLY, BASED ON THE CURRENT O.D.O.T. STANDARDS AND SUGGESTED CONSTRUCTION SEQUENCE FOR THIS PROJECT. THESE ESTIMATED SIGN DAY QUANTITIES MAY CHANGE AS THE PROJECT'S CONSTRUCTION TRAFFIC CONTROL IS MODIFIED DURING CONSTRUCTION.
- (TC-85) THESE SIGNS MUST BE ON THE OKLAHOMA DEPARTMENT OF TRANSPORTATION LIST OF APPROVED CHANGEABLE MESSAGE SIGNS. FOR A LIST OF THE APPROVED SIGNS GO TO THE OKLAHOMA DEPARTMENT OF TRANSPORTATION WEBSITE AT: http://www.okladot.state.ok.us/traffic/qpl/index.php

### SPECIAL NOTES

- (SP-1) THIS ITEM INCLUDES AN ESTIMATED 8740 L.F. (6" WIDE) WHITE AND 4245 L.F. (6" WIDE) YELLOW STRIPE. THE CONTRACTOR SHALL PROVIDE AND INSTALL AN O.D.O.T. APPROVED REMOVABLE PAVEMENT MARKING TAPE. COST FOR REMOVAL OF THIS TAPE SHALL BE INCLUDED IN THE PRICE BID FOR THIS ITEM. NON-REMOVABLE MARKING TAPE (FOIL BACK) SHALL NOT BE CONSIDERED AN APPROVED EQUAL FOR THIS ITEM.
- ⚠(SP-2) PORTABLE CHANGEABLE MESSAGE SIGN(S) SHALL BE IN PLACE 14 DAYS PRIOR TO CONSTRUCTION.
- ALL CONSTRUCTION TRAFFIC CONTROL WILL BE IMPLEMENTED ACCORDING TO CONSTRUCTION PLANS, AND INSTALLED IN A MAINIFF APPROVED BY THE ENGINEER IN ACCORDING WITH CHAPTER VIOLE THE MAINIAL BEING INSTALLED AS PART OF THIS PROJECT.
  - (SP-4) PAY ITEM TO BE USED ON DIRECTION INDICATOR BARRICADES, DRUMS, AND CHANNELIZER CONES. WARNING LIGHTS(TYPE D) SHALL NOT BE USED ON THE INSIDE OF A CURVE.
  - ⚠(SP-5) PAY ITEM TO BE USED ON WARNING SIGNS AND CONSTRUCTION BARRICADES(TYPE III).

	REVISIONS	
REV. NO.	DESCRIPTION	DATE
Δ	REVISED NOTE NUMBER	6/30/23
$\triangle$	REVISED CATEGORY	6/30/23
<u> </u>	REMOVED PAY ITEM	6/30/23
$\boxed{\mathbb{A}}$	REVISED QUANTITY	6/30/23
ß	ADDED NOTE(S)	6/30/23
	ADDED PAY ITEM	6/30/23
A	REVISED TEXT	6/30/23
	REMOVED NOTE(S)	6/30/23

			PAY QUANTITY SCHEDULE				
	0301 TRA	FFIC CON	TROL				
	PAY ITEM	CODE NO.	DESCRIPTION		UNIT	QUANTITY	
Ī	857(C)	9405	REMOVABLE PAVEMENT MARKING TAPE(6" WIDE)	TC-21), (SP-1)	LF	12,985.00	ß
ſ	871(B)	2300	CONST.ZONE IMPACT ATTEN.	(TC-52)	SD	810.00	1
	876(A)	3210	(PL)TRUCK MOUNTED ATTENUATOR (*	TC-52,76,77)	SD	50.00	4
. [	877(B)	4300	DELIVER PORTABLE LONGITUDINAL BARRIER	(TC-1)	LF	13,495.00	1
3	877(D)	4500	(PL) PORTABLE LONGITUDINAL BARRIER DELINEATOR	SD	40,500.00	1	
Ī	880(A)	6220	ARROW DISPLAY(TYPE C)	SD	48.00	1	
	880(B)	6300	CONSTRUCTION SIGNS 0 TO 6.25 SF	(TC-26, 33)	SD	943.00	1
	880(B)	6310	CONSTRUCTION SIGNS 6.26 SF TO 15.99 SF	TC-26,33,84)	SD	9,529.00	1/2
	880(B)	6320	CONSTRUCTION SIGNS 16.0 SF TO 32.99 SF (TC	-26,30,33,84)	SD	4,358.00	
	880(C)	6410	CONSTRUCTION BARRICADES(TYPE III)	(TC-26)	SD	39.00	7
	880(C)	6435	DIRECTION INDICATOR BARRICADE	SD	14.00		
	880(E)	6607	WARNING LIGHTS(TYPE B) (T	C-26), (SP-5)	SD	3,356.00	
7	880(E)	6616	WARNING LIGHTS(TYPE D) (T	C-26), (SP-4)	SD	15,036.00	
Ī	880(F)	6700	DRUMS	(TC-26)	SD	6,323.00	
Ī	880(G)	6805	CHANNELIZER CONES	(TC-26)	SD	10,033.00	
	882(A)	8210	PORT. CHANGEABLE MESSAGE SIGN (TC-52,8	34,85), (SP-2)	SD	1,136.00	1 .

	0600 STA	KING	PAY QUANTITY SCHEDULE			
	PAY ITEM	CODE NO.	DESCRIPTION	UNIT	QUANTITY	
Ì	642(B)	3300	CONSTRUCTION STAKING LEVEL II (SP-3)	LSUM	1.00	$\Delta$

PAY QUANTITY SCHEDULE 0640 CONSTRUCTION									
PAY ITEM	CODE NO.	DESCRIPTION	UNIT	QUANTITY					
641	2110	MOBILIZATION	LSUM	1.00	⋬				



PREPARED BY:
OKLAHOMA DEPARTMENT OF TRANSPORTATION
TRAFFIC ENGINEERINGSIVISION
TRAFFIC ENGINEERINGSIVISION

KARINA BURNS, P.E.

OKLA. REG. NO. 25486

07-05-23

KARINA BURNS 25486 FF

CHECK: KJB

DIVISION 4

NOBLE COUNTY

SUMMARY OF PAY ITEMS AND NOTES (TRAFFIC CONTROL)

ENGINEER: KJB 6/23

GROUP:
EM:

STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECENO. 35154 (04) SHEETNO. AT03

	SUMMARY OF SIGN QUANTITIES FOR URBAN AREA SPECIAL SIGNS																									
					SQUARE			FLANGE	NF@45		MONOTUBE TYPE B 90' 852(D) 0320 OVHD.SN.STR.,	325 TR.,	00' 852(D) 0330 VHD.SN.STR.	NOTUBE TYPE B 15' 852(D) 0335	VHD.SN.STR., NOTUBE TYPE C 852(E) 0500	L SHAFT 60" DIA	POST SPACING	FOOTING DESIGN NO.	STRUCTRLAL CONCRETE 804	REINFORCING STEEL 804	ų ų			MOVE & RESET ST. SIGNS 805(D)	(PL) REMOVAL OF EXISTING SIGNS	(PL) TRAFFIC ITEMS
			<u>A</u> <u>A</u>		TUBE POST 4"		5		101	C	N 0 0 0	0 0	2 0	MO	o No No No No No No No No No No No No No N	DRILL	ď.	6	8		SHEET 850(A)		PANELA 50(B) OH	REMO\ EXIST.	50	(PL)
SIGN NO.	APPROXIMATE LOCATION	SIGN DESCRIPTION	PROPOSED SIGNING PLAN SHEET NUMBER	SIGN DESIGNATION	А В	A	В	А	В	С	EA E	A E	ΕA	EA	EA	FT.	FT.		C.Y.	LBS.	S.F.	S.F.	S.F.	EA	EA	EA REMARKS
1	EXISTING STRUCTURE		T001	E11-1e	_																					1.00 MODIFY SIGN AS SHOWN ON T024
2	APPROXIMATELY 25 FT WEST OF EXISTING LOCATION, 3 FT NORTH OF BARRIER WALL		T001	E5-1, E13-1P	12.00														0.27					1.00		1.00 REMOVE AND RESET EXISTING E5-1, REMOVE E13-1P
3	EXISTING STRUCTURE	SS-1, SS-2	T001																				493.00	1.00	1.00	EXISTING "COUNCIL RD" EXIT DIRECTION SIGN TO BE MOVED TO THE RIGHT ON STRUCTURE
4 5	EXISTING STRUCTURE EXISTING STRUCTURE	SS-3, SS-4	T001 T001									_											359.90		1.00	2.00 MODIFY SIGNS AS SHOWN ON T024
6		SS-3, SS-4 SS-5	T001	<del> </del>		20.00	20.00			<del>                                     </del>		+	+		+		17.50	KC-3	1 98	632.00		175.00	JUB. BU		1.00	
7	APPROXIMATELY 1800 FT EAST FROM SIGN NO. 6	SS-6	T002			20.00	20.00	20.50	20.50								16.90	KC-6	3.64	930.00		240.00			1.00	
8		SS-7	T002									_											230.00		1.00	
9	EXISTING LOCATION	SS-8	T002	E5-1	12.00														0.27			30.00			1.00	
10	MACARTHUR BLVD BRIDGE	SS-9	T002	I-5																	6.25		208.00	1.00	1.00	1.00 MODIFY SIGN AS SHOWN ON T024. RESET SIGN AND INSTALL SPECIAL SIGN 9 ON NEW BRIDGE MOUNTED STRUCTURE.
11	MACARTHUR BLVD BRIDGE	SS-9A	T002																				208.00		1.00	SS-9A TO BE MOUNTED ON NEW BRIDGE MOUNTED STRUCTURE
12		SS-8	T002	E5-1	12.00														0.27			30.00			1.00	
12A	APPROXIMATELY 525 FT EAST FROM MACARTHUR BLVD BRIDGE	55-10A	T002			20.50	20.50										11.50	KC-3	1.98	632.00		127.50				
13	EXISTING LOCATION	SS-8	T002	E5-1	12.00														0.27			30.00			1.00	
13A			T002									_													1.00	REMOVE EXISTING SIGN
14	TO BE FIELD VERIFIED. AT THE UPSTREAM END OF THE PAINTED GORE	SS-10	T002												1.00	36.00							253.00			OVERHEAD STRUCTURE 1
15	EXISTING LOCATION EXISTING LOCATION	SS-52 SS-53	T002 T002	I-5					28.50									KC-6		1395.00	6.25				1.00	I-5 TO BE MOUNTED ON TOP OF SS-52
16 17	EXISTING LOCATION  EXISTING STRUCTURE	SS-03 SS-11	T002	I-5				26.50	26.50	26.50		-					7.00	KC-6	5.46	1395.00	6.25	312.00	215.25		1.00	I-5 TO BE MOUNTED ON TOP OF SS-11
18		SS-8	T003	E5-1	12.00							_					-		0.27		0.20	30.00	210.20		1.00	TO TO BE INICONTED ON TOT OF SO TI
19		SS-12	T003																				215.00		1.00	
20	EXISTING LOCATION	SS-8	T003	E5-1	12.00														0.27			30.00			1.00	
21	MERIDIAN AVE BRIDGE	SS-13	T003																				196.00	1.00	1.00	RESET "JCT I-44 1 MILE" SIGN AND INSTALL SPECIAL SIGN 13 ON NEW BRIDGE MOUNTED STRUCTURE.
22	MERIDIAN AVE BRIDGE	SS-14, SS-14A	T003	I-5																	6.25		366.00		2.00	I-5 TO BE MOUNTED ON TOP OF SS-14A. SS-14 AND SS-14A TO BE MOUNTED ON NEW BRIDGE MOUNTED STRUCTURE
23		SS-8	T003	E5-1	12.00														0.27			30.00			1.00	
24	EXISTING LOCATION  APPROXIMATELY 1870 FT EAST OF	SS-8	T003	E5-1	12.00	-						+	-+		-	-			0.27			30.00			1.00	
25	MERIDIAN AVE BRIDGE  APPROXIMATELY 1630 FT EAST OF	SS-15	T003	I-5				25.00	22.50	22.50		$\perp$					7.00	KC-6	5.46	1395.00	6.25	210.00			1.00	I-5 TO BE MOUNTED ON TOP OF SS-15
26	MERIDIAN AVE BRIDGE  APPROXIMATELY 512 FT WEST OF	SS-13	T003					22.5	22.5								17.20	KC-6	3.64	930.00		196.00			1.00	
27	27A  EXISTING LOCATION	SS-16	T003					29.5	27.0	27.0		$\perp$					7.00	KC-6	5.46	1395.00		278.50			1.00	REMOVE EXISTING SIGN
27A	APPROXIMATELY 512 FT WEST OF		T003	-		-						_	-									-			1.00	REMOVE ENSTING SIGN
28 28A	28A EXISTING LOCATION	SS-17	T003					26.00	26.00								11.20	KC-6	3.64	930.00		181.30			1.00	DEMOVE EMETING CION
28A 29	EXISTING LOCATION  EXISTING LOCATION	<del>                                     </del>	T003	<del>                                     </del>		-				-		+	-				-+					<del>                                     </del>			1.00	REMOVE EXISTING SIGN REMOVE EXISTING SIGN
30	EXISTING LOCATION  EXISTING LOCATION	SS-18	T003	<del>                                     </del>				26.50	24.00	24.00		+	_				7.00	KC-6	5.46	1395.00		277.50			1.00	TEMOVE EMOTING GIGIN
31	EXISTING LOCATION	SS-19	T003						23.50								13.00		3.64			175.00			1.00	
32		SS-20	T004	E5-1	12.00														0.27			52.50			1.00	
33	APPROXIMATELY 25 FT NORTH OF EXISTING STRUCTURE	SS-21	T004	I-5								1.	00			72.00					6.25		755.00			OVERHEAD STRUCTURE 2, EXISTING OVERHEAD SIGNS TO BE REMOVED WITH STRUCTURE, I-5 TO BE MOUNTED ON TOP OF SS-21
34	EXISTING STRUCTURE		T004																					1.00		REMOVE OVERHEAD STRUCTURE, EXISTING OVERHEAD SIGN TO BE REMOVED AND INSTALLED ON OVERHEAD STRUCTURE 3
34A	APPROXIMATELY 25 FT SOUTH OF EXISTING STRUCTURE		T004												1.00	36.00										OVERHEAD STRUCTURE 3, EXISTING OVERHEAD SIGN AT 34 TO BE MOUNTED ON OVERHEAD STRUCUTRE
35	EXISTING STRUCTURE	SS-21A	T004																				173.80		2.00	

		REVISIONS	
	REV. NO.	DESCRIPTION	DATE
1	Δ	REVISED QUANTITY	6/26/23
1	A	REVISED TEXT	6/28/23
	A	ADDED TEXT	6/28/23
	A	REMOVED COLUMN	6/28/23
	<u> </u>	ADDED COLUMN	6/28/23



CHECK: KJB 6/23

SIGN SUMMARY (SHEET 1 OF 2)

ENGINEER: KJB 6/23

STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECENO. 35154 (04) SHEETINO. AT04

										OlviiviAi		-	_	-			BAN AR	A SF	LCIAL 3	IGNS	1					1	
						_		WIDE F	LANGE PC	ST	STR.	0) 0320 N.STR.,	E TYPE B 0) 0325	MONOTUBE TYPE B 100' 852(D) 0330	N.S.I.K., E. TYPE B D) 0335	A.STR., E TYPE C 0500	T 60" DIA	: I 🛱	rrlal TE 804	ORCING 804				RESET INS 805(D)	REMOVAL OF ISTING SIGNS	IC ITEMS	
					00114	A DE	@31			645	를 다 다	852(E	TUBE 852(0	TUB!	TUB!	TUBE	SHAF		RUCI	NFO I		CICNIADE		VE & SIGN	REMO	TRAFFI	
		A	<u> </u>		SQUA TUBE F	POST	8"WF			10"WF	8	9 9	MONO 98	MONO 100'	MONO 105	MONON 8	DRILL SH	FOOTING	STRUCTRL CONCRETE	REINF	SHEET 850(A)		PANEL 850(B) OH	REMOV EXIST.	(PL) F EXIS	(PL) TF	
APPROXIMATE LOCAT	SIGN DESCRIPTION	PROP	OSED 3 PLAN EET	SIGN DESIGNATION	A	В	А	В	A	В			EA	EA	EA	EA	FT. F	г.	C.Y.	LBS.	S.F.	S.F.	S.F.	EA	EA	EA	REMARKS
APPROXIMATELY 81 FT NO EXISTING OVERHEAD STRI															1.00		72.00						829.05				OVERHEAD STRUCTURE 4
EXISTING STRUCTUR		TOO	04																								REMOVE OVERHEAD STRUCTURE, EXISTING OVERHEAD SIGNS TO BE REMOVED WITH STRUCTURE
APPROXIMATELY 930 FT S FROM SIGN NO. 35	OUTH SS-24	TOO	04	I-5								1	1.00				72.00				6.25		803.50				OVERHEAD STRUCTURE 5, I-5 TO BE MOUNTED ON TOP OF SS-24
EXISTING LOCATION EXISTING LOCATION	SS-26		04 04	E5-1	12.00														0.27			52.50			1.00 1.00		REMOVE EXISTING SIGN
APPROXIMATELY 571 FT NO EXISTING OVERHEAD STRU		тос	05												1.00		72.00						763.50				OVERHEAD STRUCTURE 6
EXISTING STRUCTUR	:	тос	05																								REMOVE OVERHEAD STRUCTURE, EXISTING OVERHEAD SIGNS TO BE REMOVED WITH STRUCTURE
EXISTING LOCATION EXISTING LOCATION		TOO	05 05	E5-1	12.00					_									0.27			52.50			1.00		REMOVE EXISTING SIGN
EXISTING LOCATION	SS-26		05	E5-1	12.00														0.27			52.50			1.00		
APPROXIMATELY 225 FT NO EXISTING STRUCTUR		TOO	05													1.00	36.00						194.00				OVERHEAD STRUCTURE 7
EXISTING STRUCTUR		TOO																									REMOVE OVERHEAD STRUCTURE, EXISTING OVERHEAD SIGN TO BE REMOVED WITH STRUCTURE
EXISTING LOCATION APPROXIMATELY 115 FT SC	JTH OF	TOO													1.00		72.00						763.50		1.00		REMOVE "TO H.E. BAILEY TURNPIKE USE I-44 WEST" SIGN OVERHEAD STRUCTURE 8
EXISTING OVERHEAD STRUCTUR		TOO	05																								REMOVE OVERHEAD STRUCTURE, EXISTING OVERHEAD SIGNS TO BE REMOVED WITH STRUCTURE
EXISTING LOCATION EXISTING LOCATION			05 05	E5-1, E13-1P W13-6	14.00														0.27		15.00	49.70			1.00		W13-6 SHOULD SPECIFY 25 MPH
APPROXIMATELY 25 FT NOF FROM EXISTING STRUC	THWEST SS-30, SS-3			I-5										1.00			72.00				6.25		585.75		1.00		WISSO FIGURE 9, EXISTING OVERHEAD SIGNS TO BE REMOVE WITH STRUCTURE, I-5 TO BE MOUNTED ON TOP OF SS-30
EXISTING LOCATION	SS-33		05	E5-1	12.00														0.27			40.00			1.00		
I-44 WESTBOUND BRID		TOO	05 05																				133.80		1.00		SS-34 TO BE MOUNTED ON NEW BRIDGE MOUNTED STRUCTURE REMOVE "WILL ROGERS WORLD AIRPORT EXITS 116A - 116B" SIGN
EXISTING STRUCTUR EXISTING LOCATION		TOO	05 05																				202.80		1.00		REMOVE "H.E. BAILEY TURNPIKE NEXT RIGHT" SIGN
APPROXIMATELY 25 FT EA EXISTING STRUCTUR		TOO	05													1.00	36.00						156.30				OVERHEAD STRUCTURE 10, EXISTING OVERHEAD SIGN TO BE REMOVE WITH STRUCTURE
EXISTING LOCATION	SS-37	T00		E5-1, E13-1P	14.00														0.27		C 05	52.20	457.00		1.00		LS TO BE MOUNTED ON TOP OF 00 20
EXISTING STRUCTUR EXISTING STRUCTUR		38 T00	05 06	I-5																	6.25		457.00 156.30		2.00		I-5 TO BE MOUNTED ON TOP OF SS-30 SAME OVERHEAD STRUCTURE AS 58
EXISTING STRUCTUR	SS-30, SS-35 Z		06	I-5																	6.25		585.75		3.00		SAME OVERHEAD STRUCTURE AS 57, I-5 TO BE MOUNTED ON TOP OF \$
EXISTING LOCATION APPROXIMATELY 345 FT EA	SS-41		06	E5-1	12.00														0.27			37.50			1.00		
SIGN NO. 59	SS-39	TOO					<b>A</b>	<b>A</b>	21.00 2	3.50							12	40 KC	6 3.64	930.00		152.50					
EXISTING LOCATION EXISTING STRUCTUR		43 T00	06	E5-1, E13-1P																			352.30		2.00		EXISTING E5-1 TO REMAIN, REMOVE E13-1P
APPROXIMATELY 25 FT W EXISTING STRUCTUR		TOO	07													1.00	36.00						153.00				OVERHEAD STRUCTURE 12, EXISTING OVERHEAD SIGN TO BE REMOVE WITH STRUCTURE
EXISTING STRUCTUR EXISTING LOCATION	SS-45	TOO		E5-1, E13-1P		_																	148.80		1.00		EXISTING E5-1 TO REMAIN, REMOVE E13-1P
EXISTING STRUCTUR EXISTING LOCATION	SS-46	TOO			12.00	_													0.27			37.50	182.50		1.00		
APPROXIMATELY 35 FT EAS	T FROM	TOO									1.	00					72.00						190.00	1.00			OVERHEAD STRUCTURE 11, EXISTING INTERCHANGE SEQUENCE SIGN T REMOVED WITH STRUCTURE, REPLACE E11-1d ON OTHER SPECIAL SIG
EXISTING LOCATION	SS-49		08	E5-1	12.00														0.27			37.50			1.00		,
EXISTING LOCATION EXISTING STRUCTUR			09 09	E5-1 E11-1d	12.00					-+			_						0.27			37.50	+		1.00	1.00	REPLACE E11-1d PANEL
EXISTING STRUCTUR		TOO	09	E11-1d																							REPLACE E11-1d PANEL
UPSTREAM END OF GO EXISTING LOCATION				E5-1 E5-1	12.00					-+								-	0.27 0.27		-	37.50 37.50	-		1.00		
EXISTING LOCATION			09	E11-1d	12.00					-+	_	_							0.21			J1.0U	+		1.00	1.00	REPLACE E11-1d PANEL
EXISTING STRUCTUR		TOO	09	E11-1d																							REPLACE E11-1d PANEL
EXISTING LOCATION EXISTING LOCATION			09	E5-1 E5-1	12.00					_									0.27 0.27			32.50 32.50			1.00		
EXISTING LOCATION EXISTING STRUCTUR			09	E11-1d	12.00	-+		-		-+		_					-+		0.21			32.30	+		1.00	1.00	REPLACE E11-1d PANEL
					280.	.00	81.0	00	6	7.50	1.	00 1	1.00	2.00	3.00	5.00	684.00		55.67	12889.00	77.50	3439.70	10330.80	6.00	62.00		

_	REV. NO.	REVISIONS DESCRIPTION	DATE
1	Δ	REVISED FOOTING DESIGN NO.	06/30/23
	A	REVISED QUANTITY	06/30/23
	<u> </u>	REVISED TOTAL QUANTITY	06/30/23
	<b>A</b>	UPDATED QUANTITY	06/30/23
	$\bigcirc$	ADDED TEXT	06/30/23
ł		REVISED TEXT	06/30/23
	$\triangle$	REMOVED COLUMN	06/30/23
	(8)	ADDED COLUMN	06/30/23

PREPARED BY:
OKLAHOMA DEPARTMENT OF TRANSPORTATION
PRAFFIC ENGINEERI SOLVISION
WWW STATEMENT OF TRANSPORTATION
PREPARED BY:
OKLAHOMA DEPARTMENT OF TRANSPORTATION
RAFFIC ENGINEERI SOLVISION
OKLAHOMA
Transportation

OKLAHOMA

OKLAHOMA

OKLA-REG. NO. 25486

DATE

OKLA-REG. NO. 25486

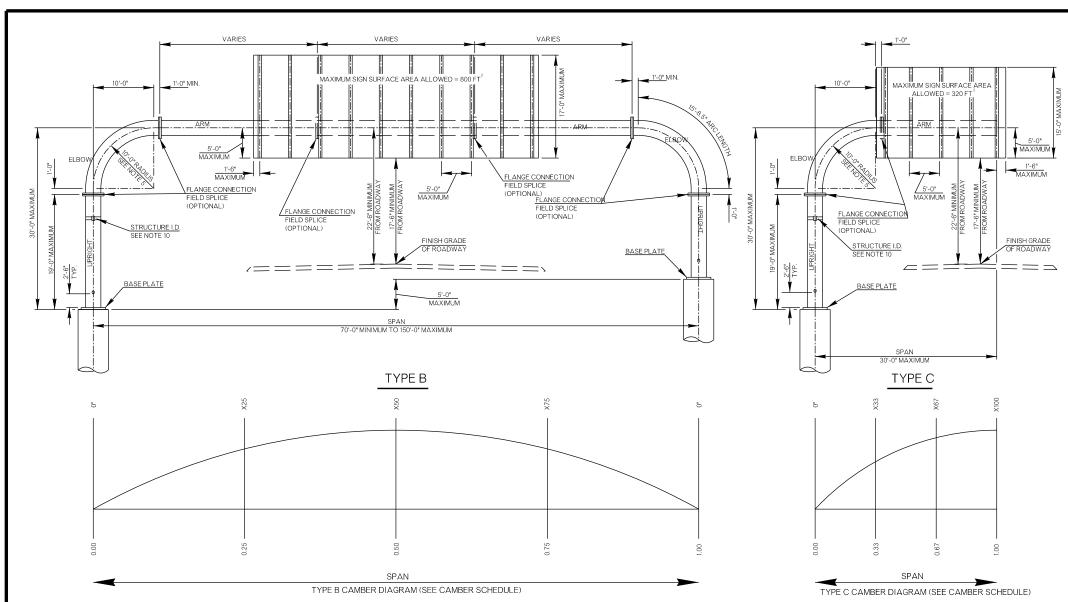
DIVISION 4

NOBLE COUNTY

ENGINEER: KJB 6/23

SIGN SUMMARY (SHEET 2 OF 2)

STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECENO. 35154 (04) SHEETINO. AT05



TYPE B CAMBER SCHEDULE						
SPAN (FT)	X25 (IN)	X50 (IN)	X75 (IN)			
70	1.08	1.27	1.08			
75	1.20	1.43	1.20			
80	1.32	1.61	1.32			
85	1.46	1.81	1.46			
90	1.61	2.03	1.61			
95	1.62	1.94	1.62			
100	1.77	2.22	1.77			
105	1.93	2.46	1.93			
110	2.10	2.72	2.10			
115	2.28	3.00	2.28			
120	2.48	3.31	2.48			
125	2.60	3.65	2.60			
130	2.92	4.01	2.92			
135	3 17	4 40	3 17			

3.43 4.83

5.29

3 71

145

3.43

3.71

TYPE C CAMBER SCHEDULE							
SPAN (FT)	X33 (IN)	X67 (IN)	X100 (IN)				
30	0.86	1.44	2.03				

# GENERAL INSTALLATION PROCEDURES

ENSURE THAT ALL ANCHOR BOLTS, BASE PLATES, AND FLANGE PLATES ARE PROPERLY ALIGNED TO PREVENT UNACCEPTABLE DISTORTION OF THE STRUCTURE UPON FINAL INSTALLATION. IN THE EVENT THAT THE DRILLED SHAFT AND ANCHOR BOLTS ARE INSTALLED PRIOR TO THE FABRICATION OF THE MONOTUBE STRUCTURE, THE MONOTUBE FABRICATOR SHOULD COORDINATE WITH THE DRILLED SHAFT CONTRACTOR TO ENSURE THAT THE BASE PLATES AND FLANGES ARE FABRICATED SO THAT PROPER ALIGNMENT OF ALL BOLT HOLES IS ACHIEVED. IN THE EVENT THAT THE MONOTUBE SIGN STRUCTURE IS FABRICATED PRIOR TO THE INSTALLATION OF THE DRILLED SHAFT AND ANCHOR BOLTS, THE DRILLED SHAFT CONTRACTOR SHOULD COORDINATE WITH THE SIGN STRUCTURE FABRICATOR TO ENSURE THAT THE ANCHOR BOLT INSTALLATION ALLOWS FOR PROPER ALIGNMENT OF ALL BOLTED CONNECTIONS. CONSTRUCTION TOLERANCES SET FORTH IN THE 2009 OKLAHOMA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION SHALL APPLY.

ERECT MONOTUBE SIGN STRUCTURE IN A MANNER APPROVED BY THE RESIDENT ENGINEER. SUPPORT ALL COMPONENTS OF THE STRUCTURE UNTIL FINAL TENSIONING OF ALL BOLTS AND FASTENERS IS COMPLETE.

INSTALLATION OF ALL FASTENERS AND BOLTS USING DIRECT TENSION INDICATORS SHALL BE IN ACCORDANCE WITH THE 2009 OKLAHOMA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION. ENSURE THAT THE MONOTUBE SIGN STRUCTURE IS PROPERLY ATTACHED TO THE ANCHOR BOLTS AND THAT ALL LEVELING NUTS ARE FLUSH WITH THE BOTTOM OF THE BASE PLATE. ENSURE THAT ALL FLANGES HAVE BEEN SECURELY FASTENED.

### GENERAL NOTES

- 1. MAXIMUM SIGN HEIGHT TO BE USED ON THE TYPE C STRUCTURE SHALL BE 15 FEET MAXIMUM SIGN HEIGHT TO BE USED ON THE TYPE B STRUCTURE SHALL BE 17 FEET
- 2. MAXIMUM SIGN AREA TO BE USED ON THE TYPE C STRUCTURE SHALL BE 320 SQUARE FEET MAXIMUM SIGN AREA TO BE USED ON THE TYPE B STRUCTURE SHALL BE 800 SQUARE FEET.
- 3. FOR SIGNS LESS THAN 10'-0" TALL, SIGNS SHALL BE CENTERED ON THE SPAN. FOR SIGNS GREATER THAN OR EQUAL TO 10'-0" TALL, BOTTOM OF SIGNS SHALL BE 5'-0" BELOW  $\mathbb Q$ . OF THE SPAN.
- 4. THE LENGTH OF THE ARM MEMBERS LABELED AS 'VARIES' SHOULD BE A MINIMUM OF 30'-0" FOR TYPE 'B' MONOTUBE SIGN STRUCTURES.
- 5. ADJUST BEND RADIUS ACCORDING TO CAMBER DIAGRAM. ALL TRANSVERSE PLATES CONNECTING TO AN ELBOW SHALL BE PERPENDICULAR TO THE CENTERLINE OF THE ELBOW AT THE LOCATION OF THE CONNECTION.
- 6. STRUCTURAL STEEL TUBING USED IN THE FABRICATION OF MONOTUBES SHALL EITHER BE COLD-FORMED WELDED OR SEAMLESS TUBING CONFORMING TO THE ASTM A500, GRADE C (MEETING AASHTO M270 ZONE 2 FRACTURE CRITICAL CHARPY V-NOTCH REQUIREMENTS) OR API 5L PSL 2, GRADE X52 (MEETING AASHTO M270 ZONE 2 FRACTURE CRITICAL CHARPY V-NOTCH REQUIREMENTS).
- 7. BASE PLATES, FLANGE PLATES, AND FILLER PLATES TO BE STRUCTURAL STEEL CONFORMING TO THE SPECIFICATIONS OF ASTM DESIGNATION: A709, GRADE 50.
- 8. ALL FLANGE BOLTS TO CONFORM TO THE SPECIFICATIONS OF ASTM A490, TYPE 1,AND SHALL BE TIGHTENED AND INSPECTED USING DIRECT TENSION INDICATORS TO CONFORM TO THE SPECIFICATIONS OF ASTM F959, TYPE 490. ALL WASHERS TO CONFORM TO THE SPECIFICATIONS OF ASTM F436, TYPE 1, ALL NUTS USED TO FASTEN ASTM A490 BOLTS SHALL BE ASTM A563, GRADE DH. ALL ANCHOR BOLTS TO CONFORM TO THE SPECIFICATIONS OF ASTM F1554-GRADE 55 (MEETING ASTM F1554 CHARPY V-NOTCH REQUIREMENTS) AND TO BE TIGHTENED AND INSPECTED USING DIRECT TENSION INDICATORS CONFORMING TO THE SPECIFICATIONS OF ASTM F2437 (TYPE 1 GRADE 55). ALL ANCHOR BOLT NUTS TO CONFORM TO THE SPECIFICATIONS OF ASTM A563-GRADE A. ALL ANCHOR BOLT WASHERS TO CONFORM TO THE SPECIFICATIONS OF ASTM F436, TYPE 1.
- 9. HOT-DIP GALVANIZE ALL TUBE MEMBERS AND PLATES PER ASTM A123. COAT ASTM A490 FASTENERS PER ASTM F1136, GRADE 3. WHEN COATING ASTM A490 FASTENERS HYDROGEN EMBRITTLEMENT SHALL BE INVESTIGATED AND PREVENTED PER THE APPLICABLE ASTM SPECIFICATIONS. COAT NUTS USED WITH ASTM A490 FASTENERS PER ASTM F1136, GRADE 5. COAT WASHERS USED WITH ASTM A490 FASTENERS PER ASTM F1136, GRADE 3. COAT ANCHOR BOLTS, NUTS USED WITH ANCHOR BOLTS, AND WASHERS USED WITH ANCHOR BOLTS PER ASTM F2329.
- 10.STAMP STRUCTURE IDENTIFICATION ON UPRIGHT OF STRUCTURE WITH THE FOLLOWING INFORMATION: JP#,TYPE 'B' OR TYPE 'C', STRUCTURE LENGTH, MAXIMUM ALLOWABLE SIGN AREA, MAXIMUM ALLOWABLE SIGN HEIGHT, DATE MANUFACTURED, AND MANUFACTURER'S NAME.
- 11.MAST ARMS TO BE TEMPORARILY SUPPORTED TO TAKE ALL LOAD OFF OF THE FIELD SPLICES WHILE BOLTS ARE BEING TIGHTENED IN ORDER TO FIRMLY SEAT THE FLANGE PLATES AND BASE PLATES.
- 12.POSTS FOR TUBULAR SIGN STRUCTURES TO BE FORMED TO THE RADII SHOWN ON THE PLANS BY FABRICATION METHODS WHICH WILL NOT CRIMP OR BUCKLE THE INTERIOR RADIUS OF THE PIPE BEND.
- 13.CLIPS, EYES OR REMOVABLE BRACKETS TO BE AFFIXED TO ALL POSTS AND MAST ARMS, AS NECESSARY, TO SECURE THE SIGN DURING SHIPPING AND FOR LIFTING AND MOVING DURING ERECTION. THIS IS TO PREVENT DAMAGE TO THE FINISHED GALVANIZED OR PAINTED SURFACES, BRACKETS ON TUBULAR SIGN STRUCTURES TO BE REMOVED AFTER ERECTION. DETAILS OF SUCH DEVICES TO BE SHOWN ON THE SHOP DRAWINGS.
- 14.BOLTS WITH DIAMETERS EXCEEDING BY UP TO 1/4 INCH THE DIAMETER OF THE BOLTS SHOWN ON THE PLANS MAY BE USED, PROVIDED THAT THE REQUIRED CLEARANCES AND EDGE DISTANCE ARE NOT REDUCED BELOW THAT REQUIRED FOR THE LARGER BOLT.
- 15.FABRICATE ALL SIGN STRUCTURES TO THE LARGEST PRACTICAL SECTIONS PRIOR TO GALVANIZING. SPLICE LOCATIONS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL AND THE CONTRACTOR SHALL NOT COMMENCE FABRICATION UNTIL SUCH SPLICE LOCATIONS ARE APPROVED.
- 16.ALL TYPE 'C' SIGN STRUCTURES TO HAVE A REMOVABLE CAP ON THE END OF THE HORIZONTAL MEMBER OF THE STRUCTURE.
- 17. WELDING OF STEEL TO CONFORM TO THE REQUIREMENTS OF AWS D1.1(LATEST REVISION). GRIND ALL AREAS TO BE WELDED TO BRIGHT METAL. COMPLETE ALL WELDING AND REQUIRED NON -DESTRUCTIVE TESTING BEFORE MATERIAL IS GALVANIZED. TEST ALL CIRCUMFERENTIAL WELDS NON- DESTRUCTIVELY USING THE ENHANCED MAGNETIC PARTICLE METHOD IN ACCORDANCE WITH ODOT STANDARD SPECIFICATION 720.03B. MAXIMUM WELD UNDERCUT SHALL BE 0.01".
- 18.ALL TUBE-TO-TRANSVERSE PLATE COMPLETE JOINT PENETRATION (CJP) GROOVE WELDS SHALL BE ULTRASONICALLY TESTED (UT) FOR CRACKS BEFORE AND AFTER GALVANIZATION.
- 19. WELD FILLER MATERIAL SHALL MEET ALL CHARPY V-NOTCH REQUIREMENTS SPECIFIED IN AWS D1.1 AT A TEMPERATURE OF  $40^\circ\mathrm{F}.$
- 20.ALL BASE METAL SHALL BE PREHEATED IN ACCORDANCE WITH AWS D1.1 PRIOR TO WELDING.
- 21.BACKING RING SHALL BE THOROUGHLY FUSED WITH THE WELD MATERIAL.
- $22. SMAW\ ELECTRODES\ SHALL\ BE\ THE\ LOW-HYDROGEN\ CLASSIFICATION\ AS\ DEFINED\ BY\ AWS\ D1.1.$
- 23.STORAGE, HANDLING, AND USE OF LOW-HYDROGEN ELECTRODES SHALL BE IN CONFORMANCE WITH AWS D1.1.
- 24.THERE SHALL BE NO POST WELD HEAT TREATMENT OF THE TUBE-TO-TRANSVERSE PLATE CONNECTION.
- 25.THE CONTRACTOR SHALL PROVIDE SHOP DRAWINGS TO ODOT BRIDGE DIVISION. A WELDING PROCEDURE SPECIFICATION (WPS) SHALL BE ATTACHED TO THE SHOP DRAWINGS.
- 26.BACKING RING MATERIAL SHALL BE IN ACCORDANCE WITH AWS D1.1.

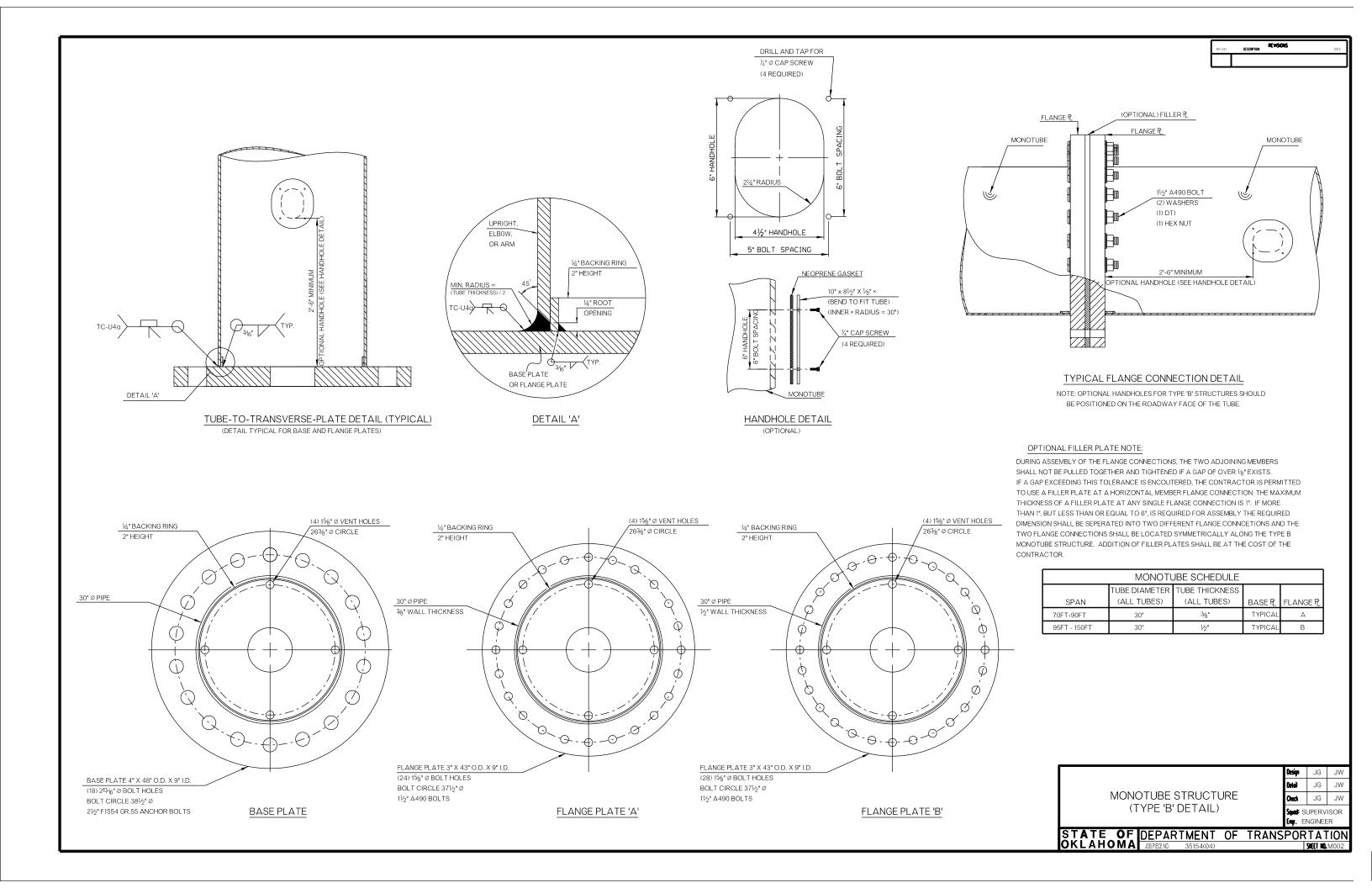
BASIS OF PAYMENT					
ITEM NO.	ITEM	UNIT			
852(D)	OVHD.SN.STR., MONOTUBE TYPE B	EΑ			
852(E)	OVHD.SN.STR., MONOTUBE TYPE C	EΑ			

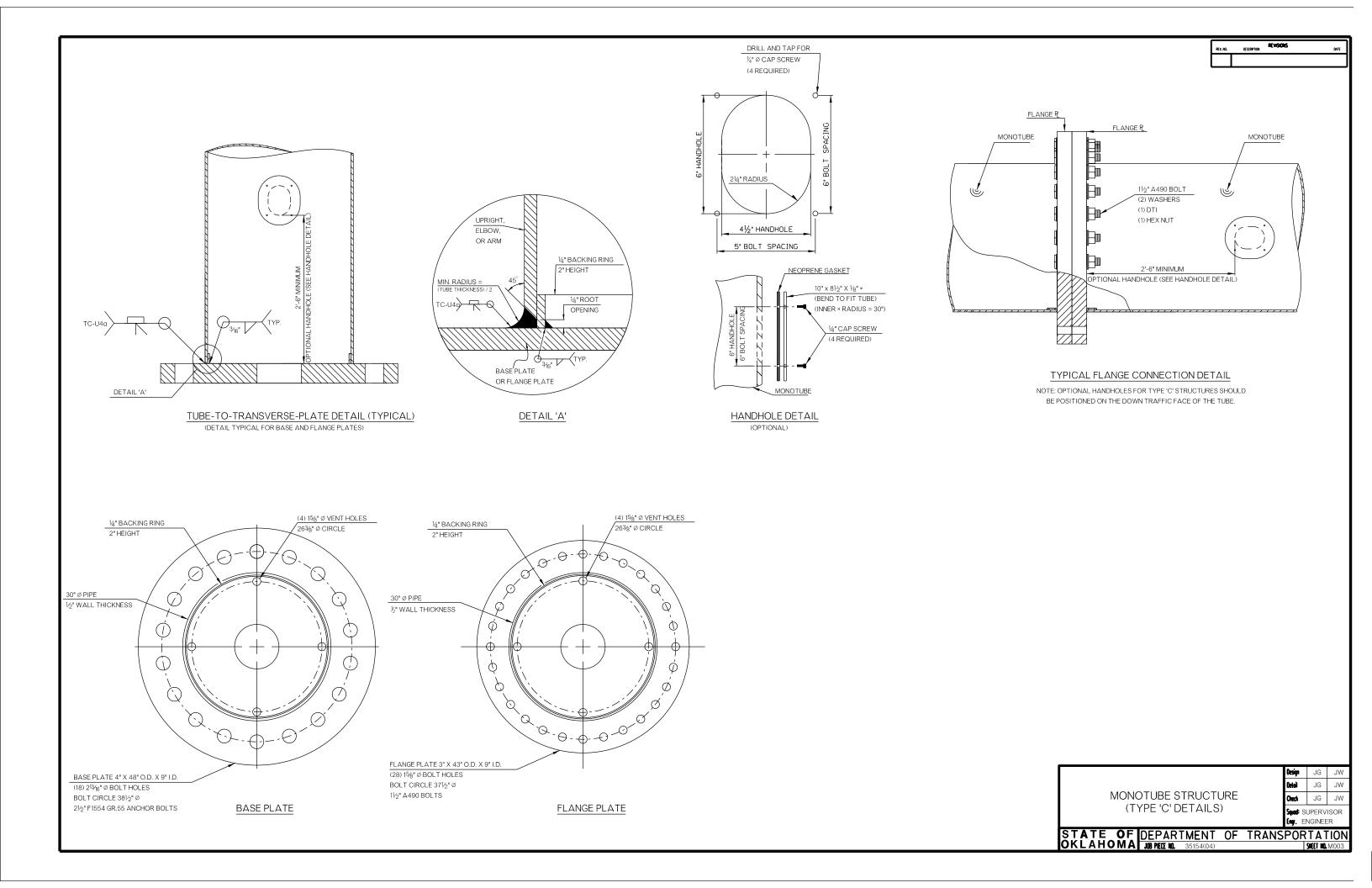


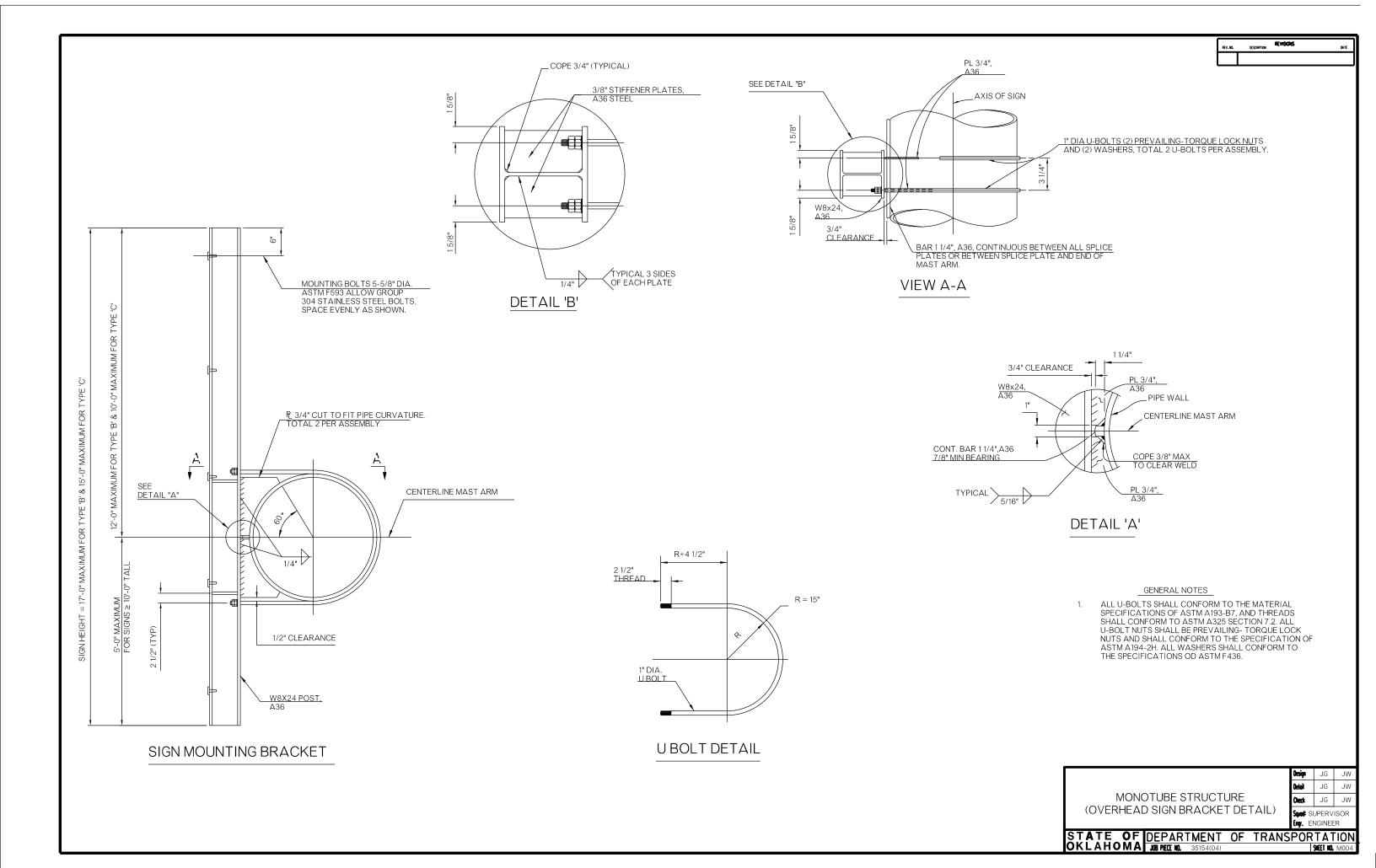
MONOTUBE STRUCTURE (TYPE 'B' & TYPE 'C')

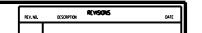
Design JG JW
Deloi JG JW
Check JG JW
Squot: SUPERVISOR
Lay. ENGINEER

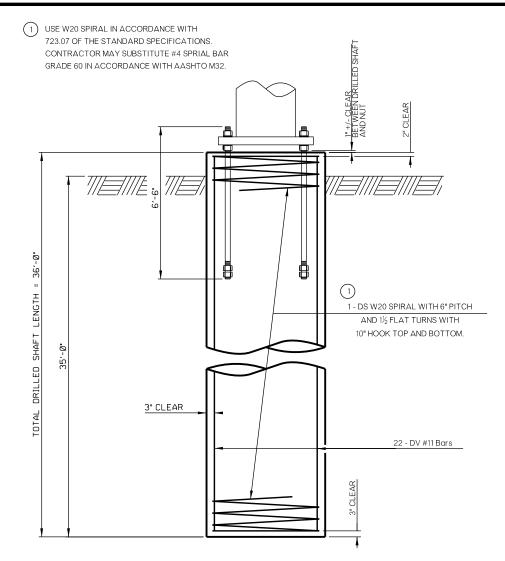
STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOB PIECE NO. 35154(04) SHEET NO. MOO1

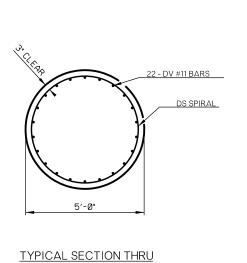




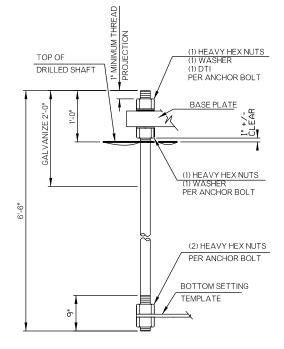








60" DRILLED SHAFT



2½" Ø ANCHOR BOLT DETAIL

# **DRILLED SHAFT NOTES:**

MATERIAL PROPERTIES

CLASS 'AA' CONCRETE = 4,000 PSI REINFORCING STEEL = 60,000 PSI

THE DRILLED SHAFT FOR THE MONOTUBE SIGN STRUCTURE HAS BEEN DESIGNED FOR THE FOLLOWING PROPERTIES:

-- COHESIVE SOIL

UNIT WEIGHT = 120 PCF COHESION = 1000 PSF

-- GRANULAR SOIL

UNIT WEIGHT = 120 PCF

INTERNAL FRICTION ANGLE = 28 DEGREES

IF SITE CONDITIONS ARE ENCOUNTERED THAT DIFFER FROM THOSE SPECIFIED ABOVE, THE ENGINEER SHALL BE CONTACTED. SUCH CONDITIONS ARE, BUT NOT LIMITED TO, AS FOLLOWS:

- -- SOIL HAS HIGH ORGANIC CONTENT OR CONSISTS OF SATURATED SILT AND CLAY.
- -- THE SITE WON'T SUPPORT THE WEIGHT OF THE DRILLING RIG.
- -- ROCK IS ENCOUNTERED.

DRILLED SHAFTS SHALL BE CONSTRUCTED ACCORDING TO THE OKLAHOMA DEPARTMENT OF TRANSPORTATION'S STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION AND ASSOCIATED SPECIAL PROVISIONS. THE USE OF THE "DOUBLE CASING METHOD" IS NOT ALLOWED FOR THIS DESIGN.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONFIRMING THAT THE LOCATION AND ELEVATION OF THE DRILLED SHAFT ARE AS REQUIRED IN THE PLANS. THE CONTRACTOR SHALL COORDINATE WITH THE MONOTUBE SUPPLIER TO ENSURE THAT THE ORIENTATION OF THE ANCHOR BOLTS IN THE DRILLED SHAFT ALLOW FOR PROPER ALIGNMENT OF ALL BASE PLATES AND FLANGES UPON FINAL INSTALLATION.

NOTE: FOR ADDITIONAL DRILLED SHAFT DETAILS,
SEE "MONOTUBE STRUCTURE (DRILLED SHAFT
DETAILS) (SHEET 3 OF 3)".
FOR DRILLED SHAFT DETAILS IN THE MEDIAN,
SEE "MONOTUBE STRUCTURE (DRILLED SHAFT
DETAILS) (SHEET 2 OF 3)".

	DRILLED SHAFT BAR LIST (INCLUDED IN CONTRACT UNIT PRICE OF DRILLED SHAFT)						
MARK SIZE NO. FORM LEN					LENGTH		
	PLAIN REINFORCING BARS						
	DS	W20	1	BNT	1,052'-9"		
	DV	#11	22	STR	35'-7"		

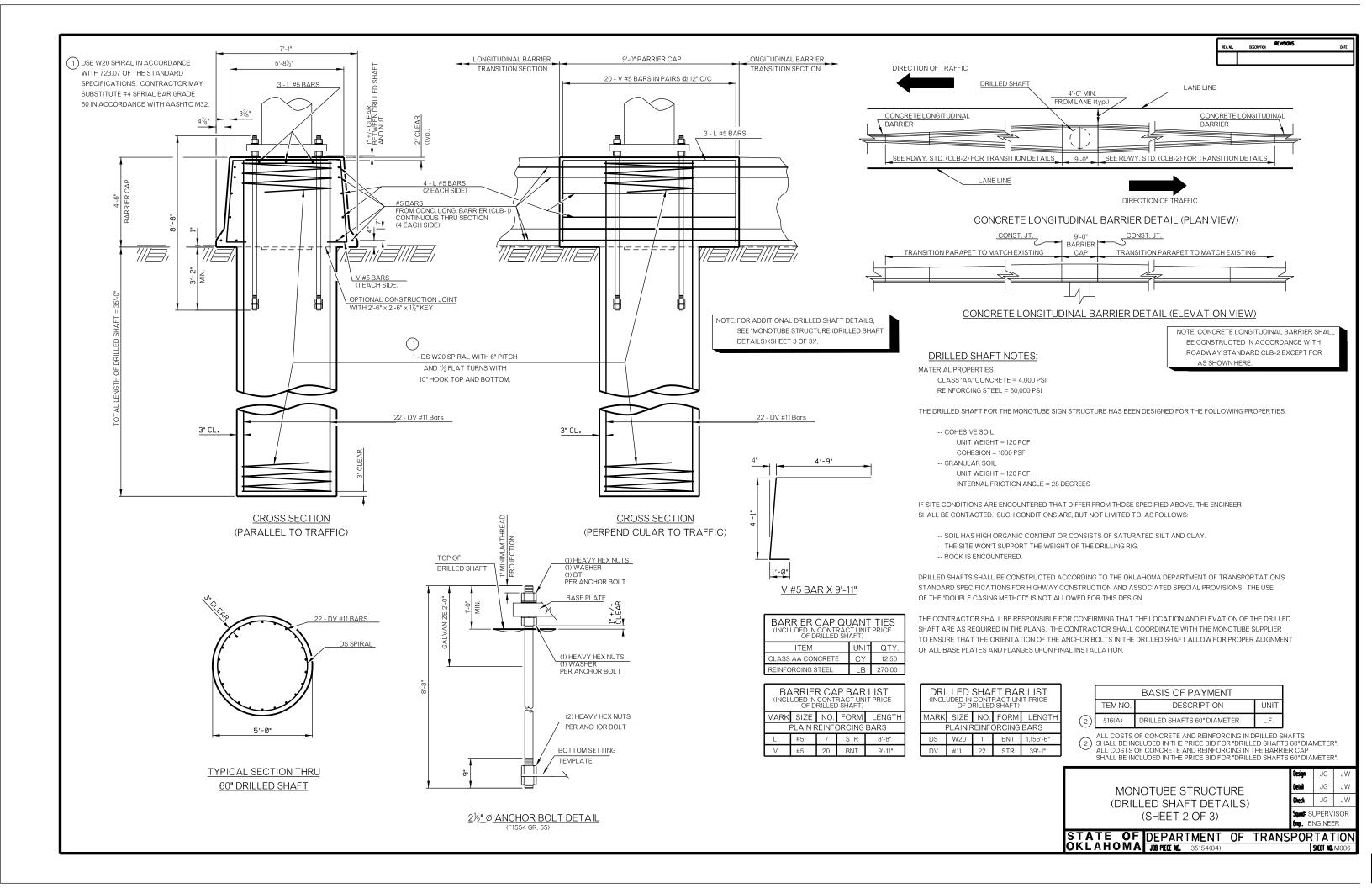
		BASIS OF PAYMENT	
	ITEM NO.	DESCRIPTION	UNIT
2)	516(A)	DRILLED SHAFTS 60" DIAMETER	L.F.

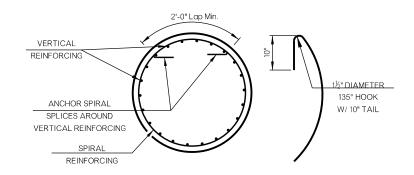
2 ALL COSTS OF CONCRETE AND REINFORCING IN DRILLED SHAFTS SHALL BE INCLUDED IN THE PRICE BID FOR "DRILLED SHAFTS 60" DIAMETER".

MONOTUBE STRUCTURE (DRILLED SHAFT DETAILS) (SHEET 1 OF 3)

_	uy.	40114EE	-11
	for FI	NGINEE	R
	Squad: St	JPERV	ISOR
	Check	JG	JW
	Deloil	JG	JW
	Design	JG	JW

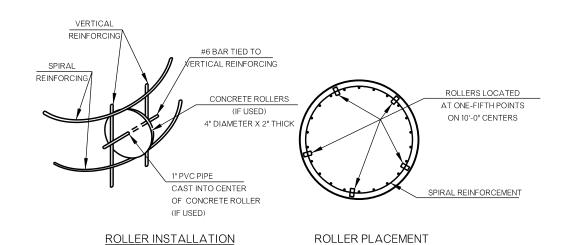
STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOB PRECE NO. 35154(04) 9962 NO. MOOS





NOTE: SPIRAL BAR LENGTH QUANTITY DOES NOT INCLUDE LAP. IF LAP IS REQUIRED, THE LENGTH OF THE LAP SHALL BE AS SHOWN.

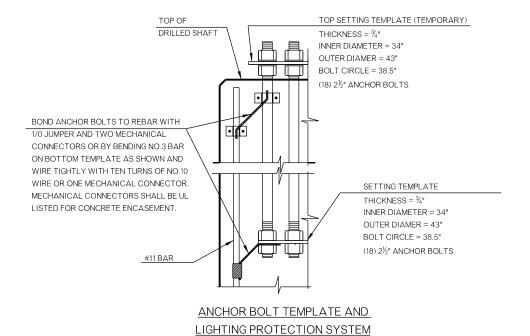
SPIRAL REINFORCING SPLICE DETAIL



# **DETAIL OF CONCRETE ROLLERS**

ROLLER PLACEMENT

NOTE: IF CONCRETE ROLLERS ARE USED, THEY SHALL HAVE A MINIMUM 28 DAY COMPRESSIVE STRENGTH OF 4,000 P.S.I.



MONOTUBE STRUCTURE (DRILLED SHAFT DETAILS) (SHEET 3 OF 3)

Delai JG Check JG JW Squat SUPERVISOR M. ENGINEER

STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOB PECE NO. 35154(04) 90021 NO. MOO7





NOT TO SCALE

KEY OVERHEAD STRUCTURE IMPACT ATTENUATOR

DIVISION 4

NOBLE COUNTY

PROPOSED SIGNING PLAN (SHEET 1 OF 9) ENGINEER: KJB 6/23 STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECENO. 35154 (04) SHEETINO. TOO1

KARINA BURNS 25486





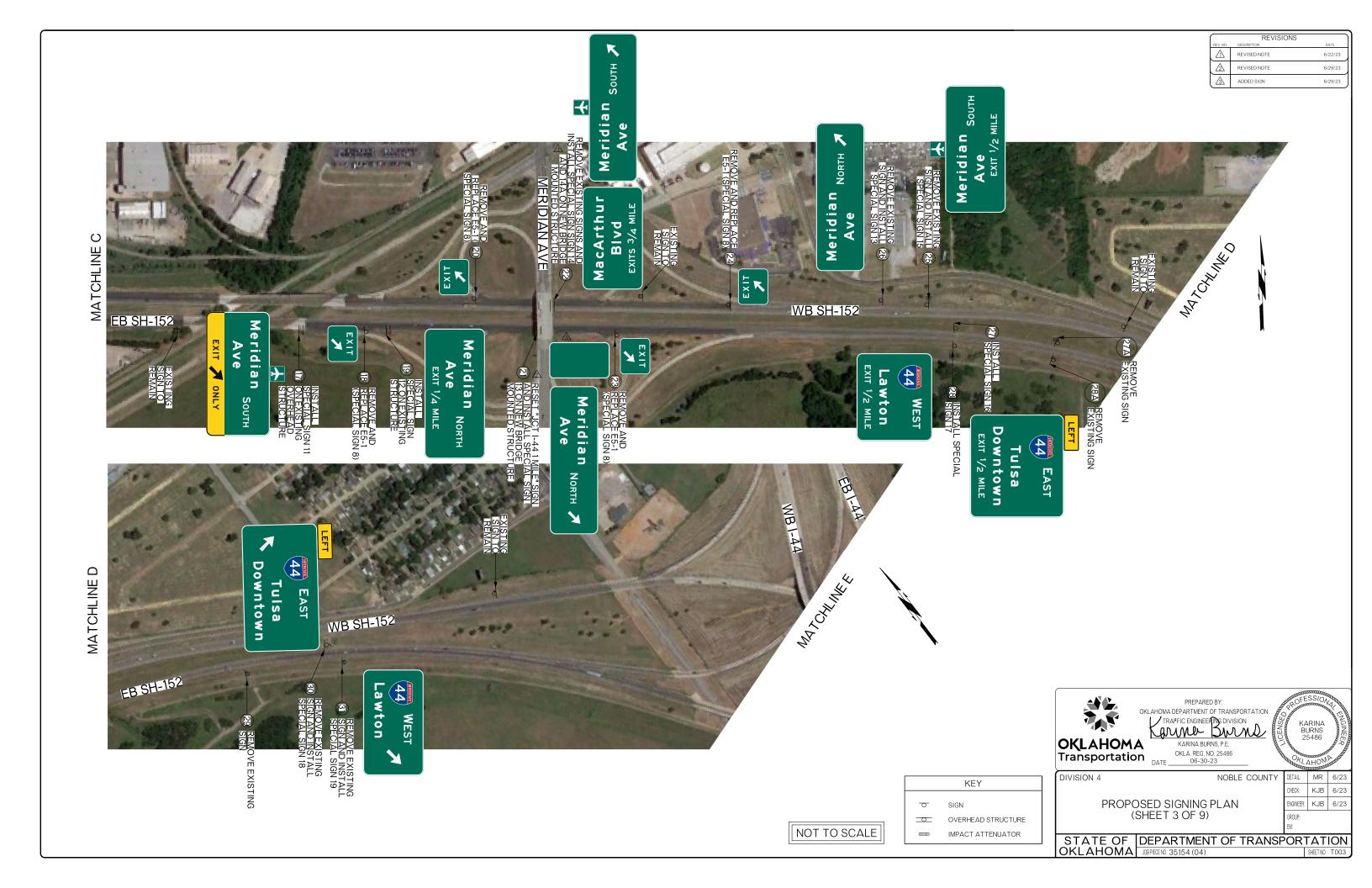
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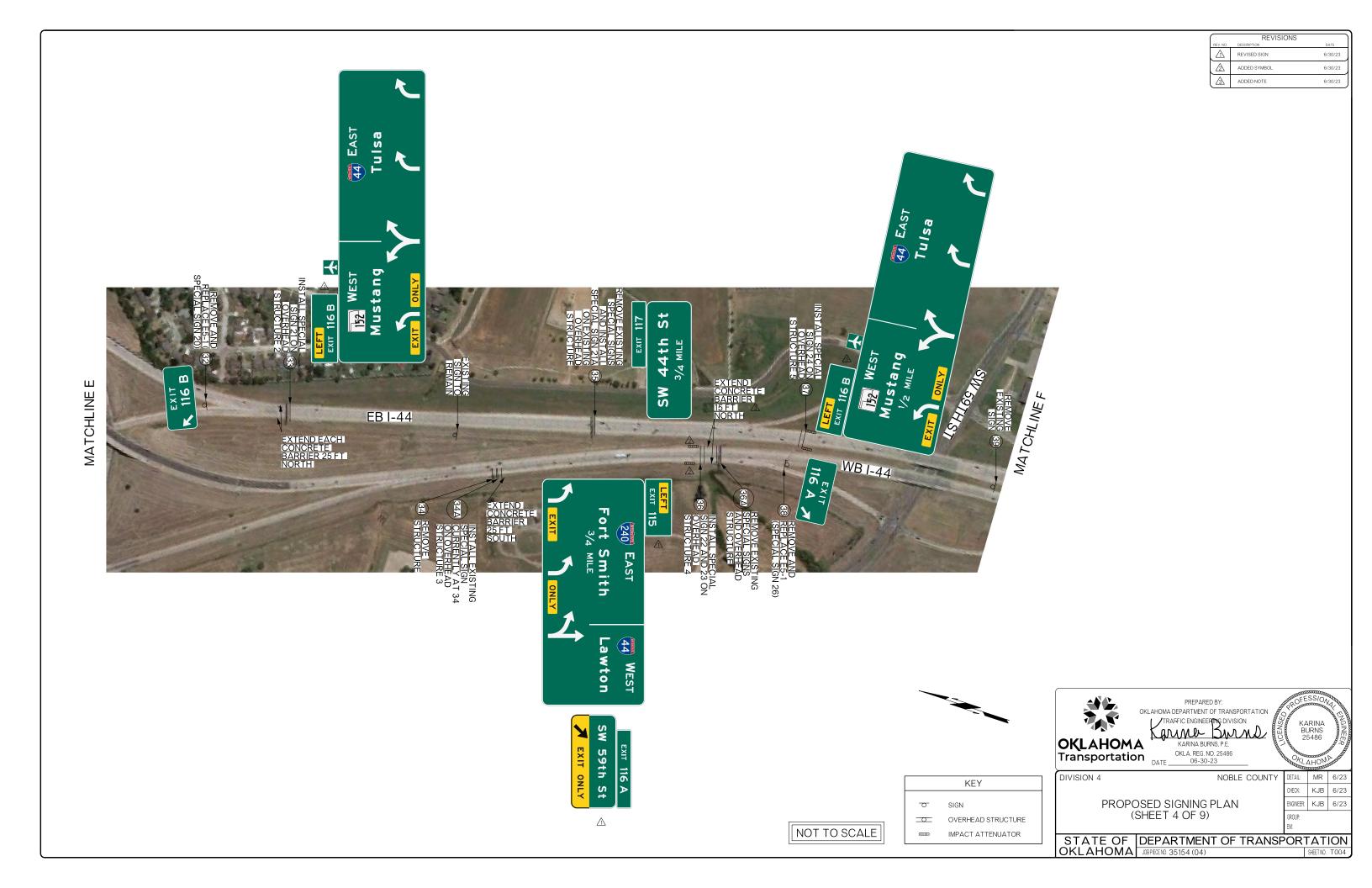
SIGN
OVERHEAD STRUCTURE
IMPACT ATTENUATOR

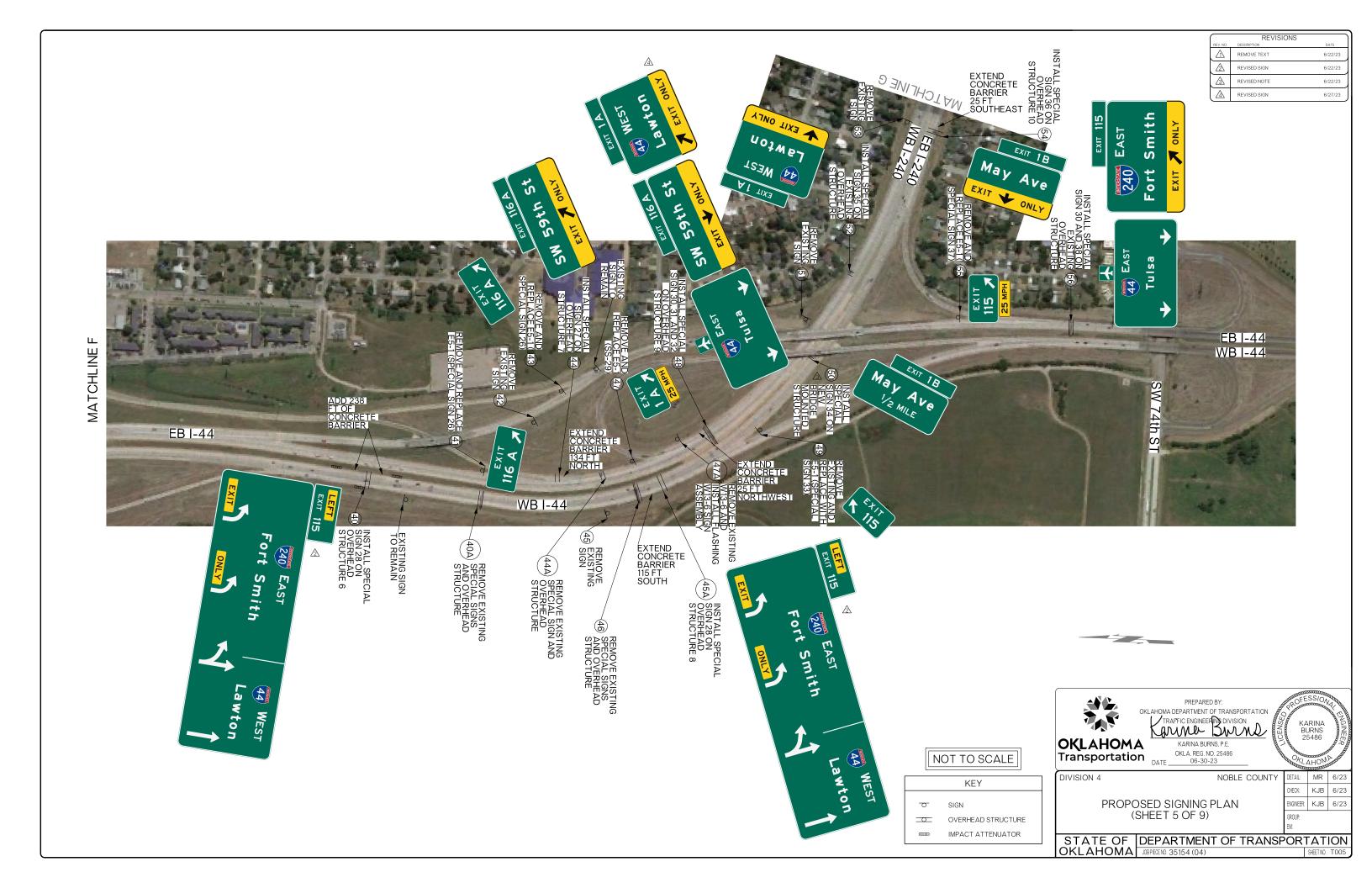


STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECE NO. 35154 (04) SHEETINO. TOO2

NOT TO SCALE













KEY

DIVISION 4

PREPARED BY:
OKLAHOMA DEPARTMENT OF TRANSPORTATION
TRAFFIC ENGINEER OF DIVISION
WWW DATE
OKLAHOMA
KARINA BURNS, P.E.
OKLA. REG. NO. 25486
DATE
06-30-23

NOBLE COUNTY PROPOSED SIGNING PLAN (SHEET 6 OF 9) ENGINEER: KJB 6/23

STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECENO. 35154 (04) SHEETNO. TOO6

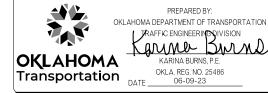
OVERHEAD STRUCTURE NOT TO SCALE IMPACT ATTENUATOR





KEY OVERHEAD STRUCTURE IMPACT ATTENUATOR

NOT TO SCALE



PREPARED BY:
OKLAHOMA DEPARTMENT OF TRANSPORTATION
TRAFFIC ENGINEERINGS IVISION

DIVISION 4 NOBLE COUNTY

PROPOSED SIGNING PLAN (SHEET 7 OF 9) ENGINEER: KJB 6/23

STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECENO. 35154 (04) SHEETINO. TOO7

REVISIONS DATE



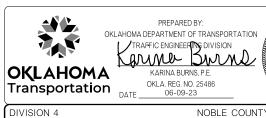


KEY

O SIGN

O OVERHEAD STRUCTURE

IMPACT ATTENUATOR



arisportation	DATE06-09-23	-		MOH	NAME OF THE PARTY
VISION 4	NOBLE	COUNTY	DETAIL:	MR	6/2
			CHECK:	KJB	6/2
PROPOSE	ED SIGNING PLAN		ENGINEER:	KJB	6/2
(SHI	EET 8 OF 9)		GROUP: EM:		
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KARINA BURNS 25486

STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECENO. 35154 (04) SHEETINO. TOOR

NOT TO SCALE

ADDED SIGNATURE





PREPARED BY:
OKLAHOMA DEPARTMENT OF TRANSPORTATION
TRAFFIC ENGINEERING DIVISION
WWW Survey
OKLAHOMA
KARINA BURNS, P.E.
OKLA. REG. NO. 25486
DATE 06-30-23

NOBLE COUNTY

ENGINEER: KJB 6/23
GROUP:

DIVISION 4

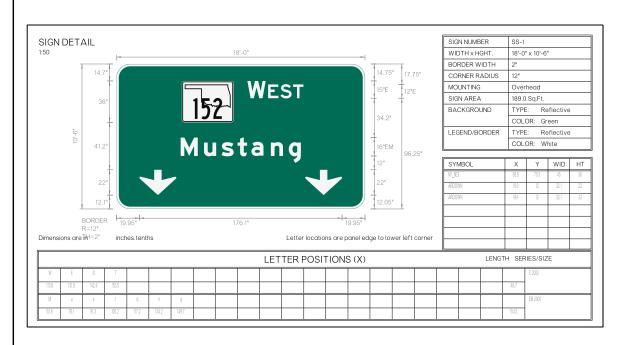
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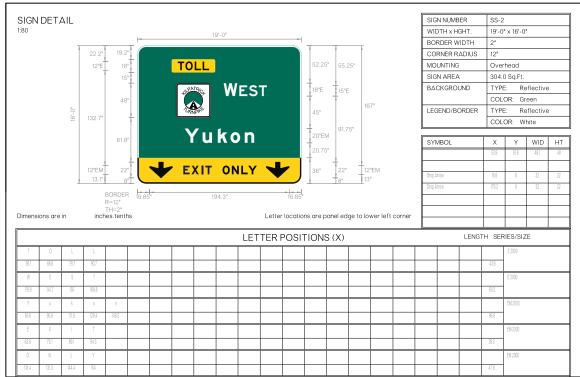
	EM:	
	DEPARTMENT OF TRANSPOR	TATION
OKLAHOMA	JOB PIECE NO. 35154 (04)	SHEET NO. TOOS

NOT TO SCALE

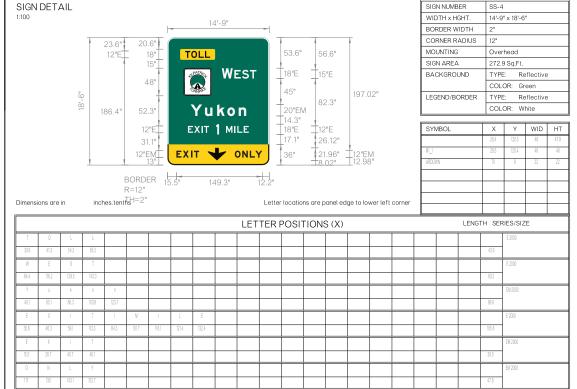
KEY							
þ	SIGN						
	OVERHEAD STRUCTURE						
	IMPACT ATTENUATOR						

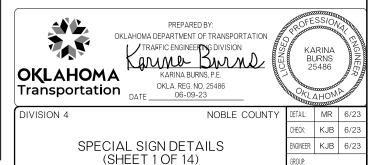
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DESCRIPTION
DATE







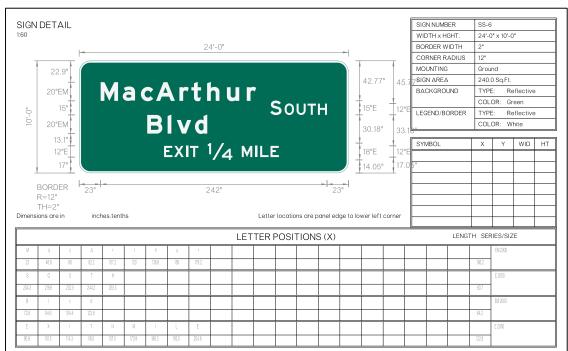


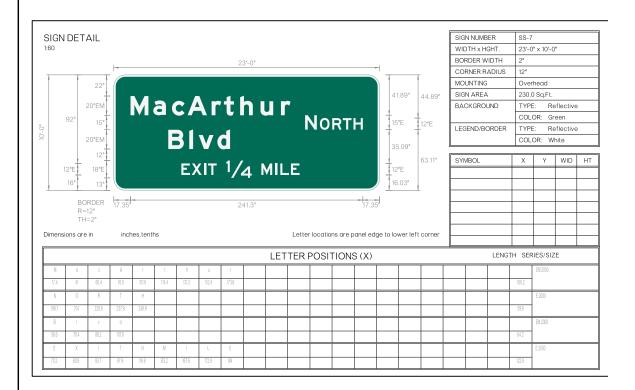


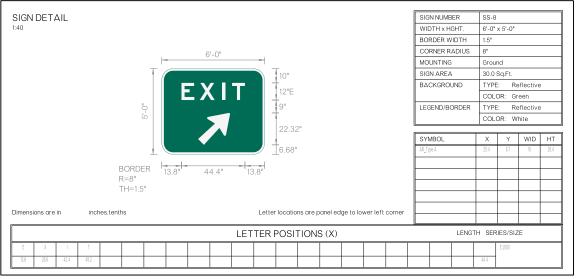
STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECENO. 35154 (04) SHEETNO. TO10

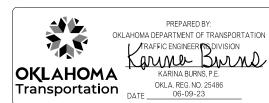
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DESCRIPTION
DATE











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06-09-23

NOBLE COUNTY

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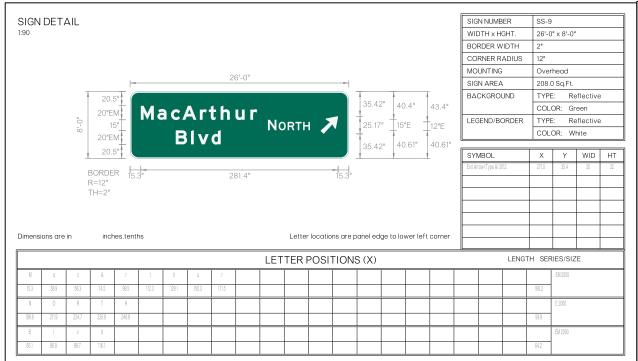
(HECK: KJB

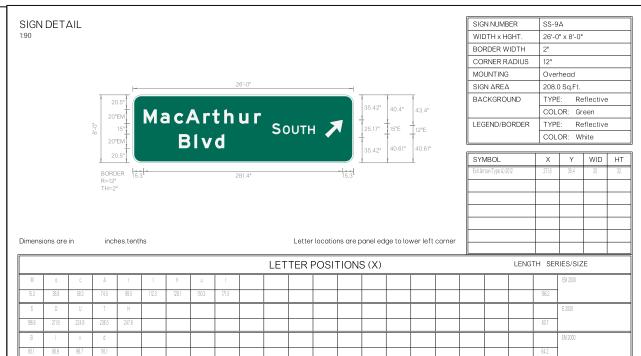
DIVISION 4

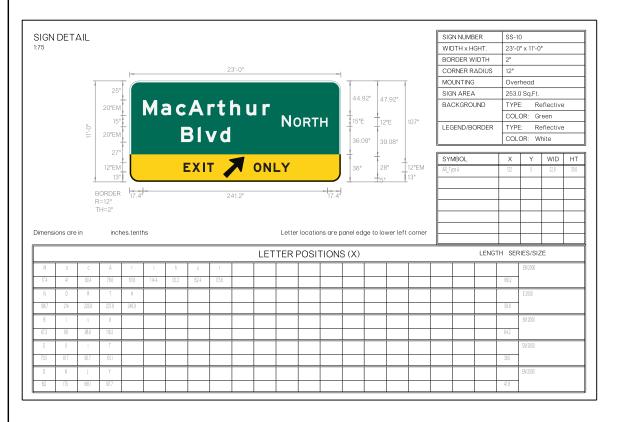
SPECIAL SIGN DETAILS (SHEET 2 OF 14) ENGINEER KJB 6/23
GROUP:
EM:
PORTATION

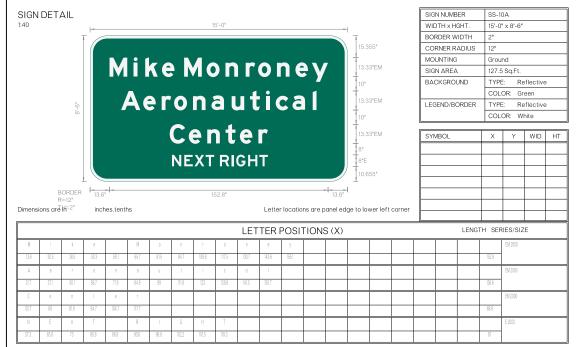
KARINA BURNS 25486

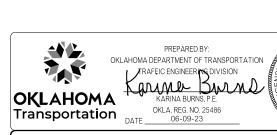
STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECENO. 35154 (04) SHEETNO. T011









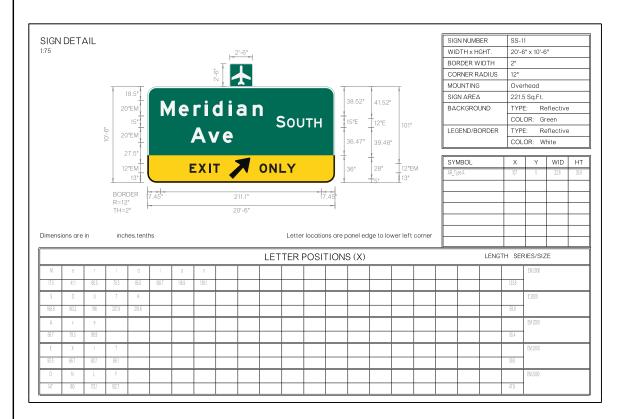


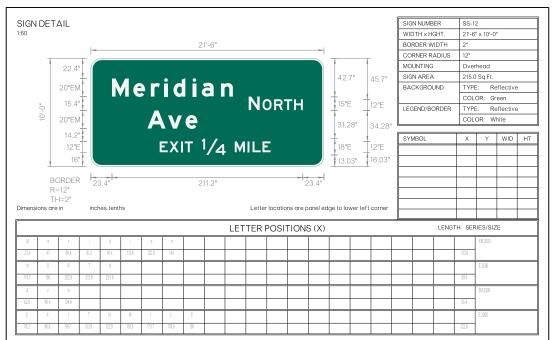
NOBLE COUNTY

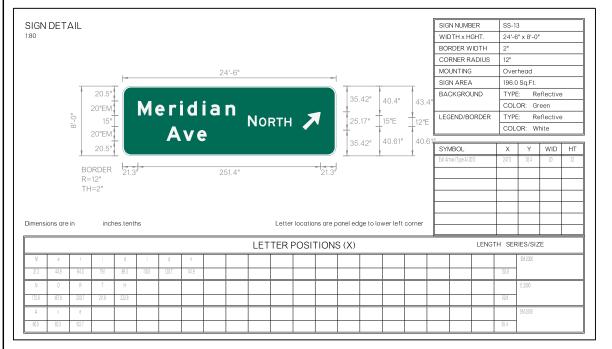
ENGINEER: KJB 6/23 SPECIAL SIGN DETAILS

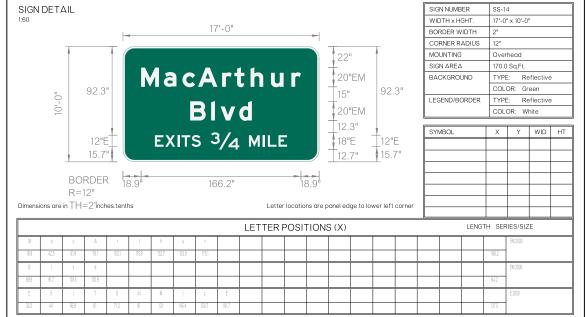
(SHEET 3 OF 14) STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECENO. 35154 (04) SHEETINO. TO12

REVISIONS











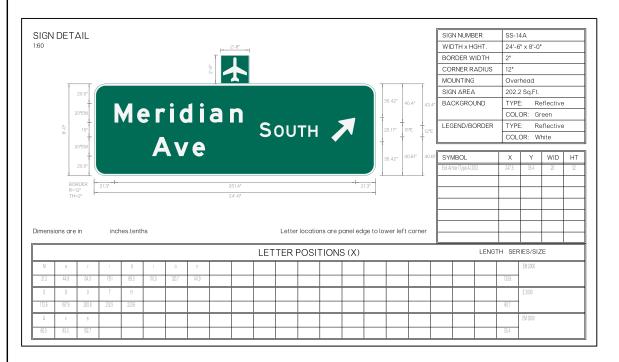
CHECK: KJB 6/23

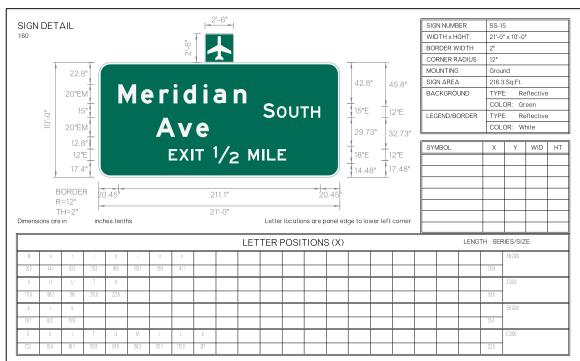
SPECIAL SIGN DETAILS (SHEET 4 OF 14)

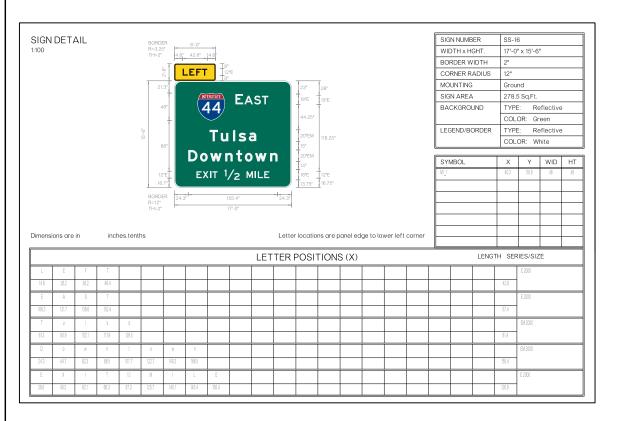
ENGINEER: KJB 6/23

STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECENO. 35154 (04) SHEETNO. T013

REVISIONS











DIVISION 4

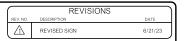
NOBLE COUNTY

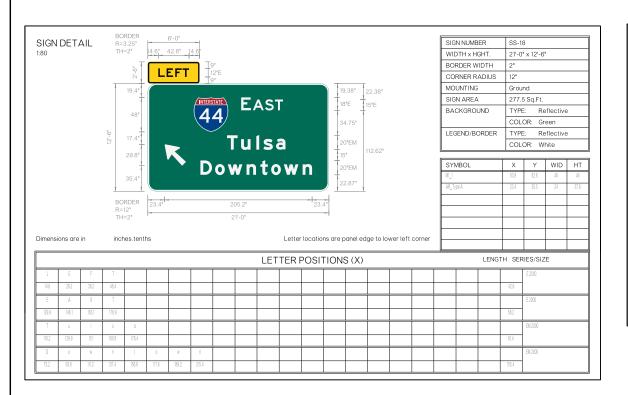
SPECIAL SIGN DETAILS (SHEET 5 OF 14)

	ENGINEER:	KJB	6/23
	GROUP:		
	EM:		
SPORTATION			

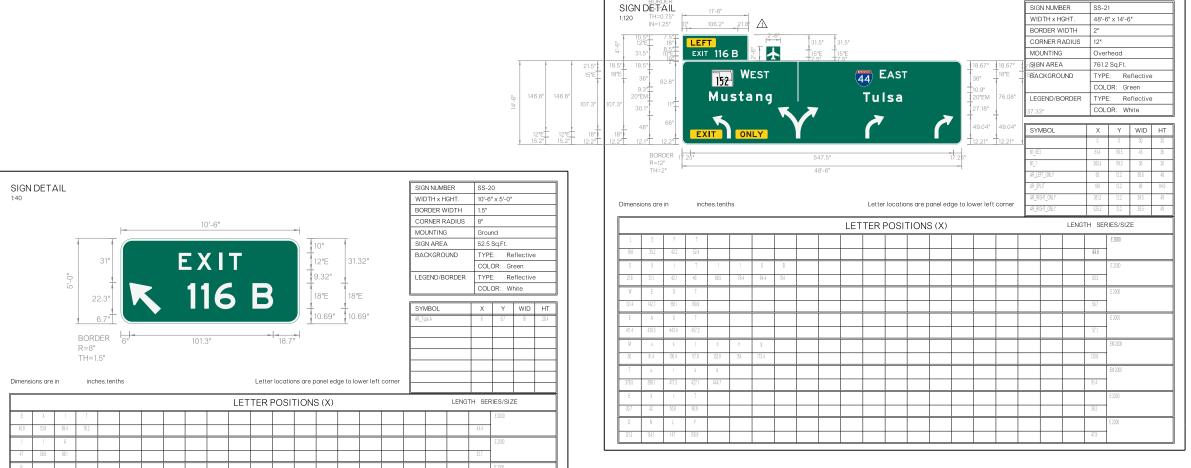
CHECK: KJB 6/23

STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECENO. 35154 (04) SHEETIND. T014











PREPARED BY: OKLAHOMA DEPARTMENT OF TRANSPORTATION TRAFFIC ENGINEERING DIVISION

KARINA BURNS, P.E.

BURNS 25486

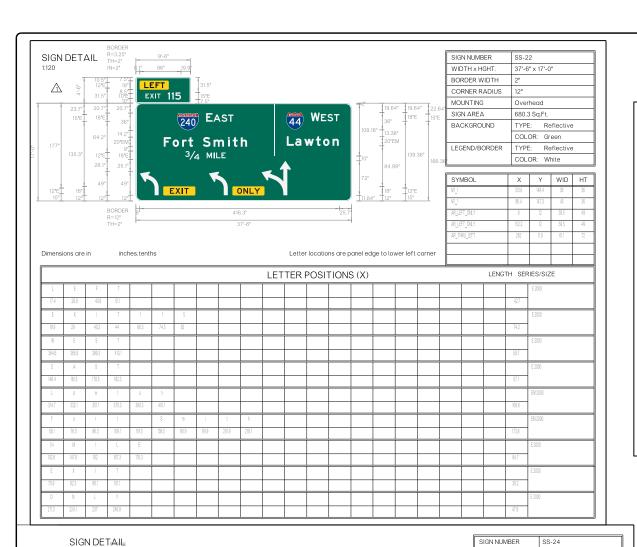
DIVISION 4

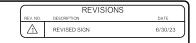
NOBLE COUNTY

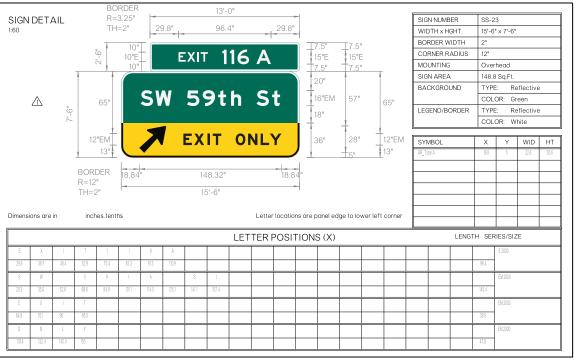
CHECK: KJB 6/23 ENGINEER: KJB 6/23

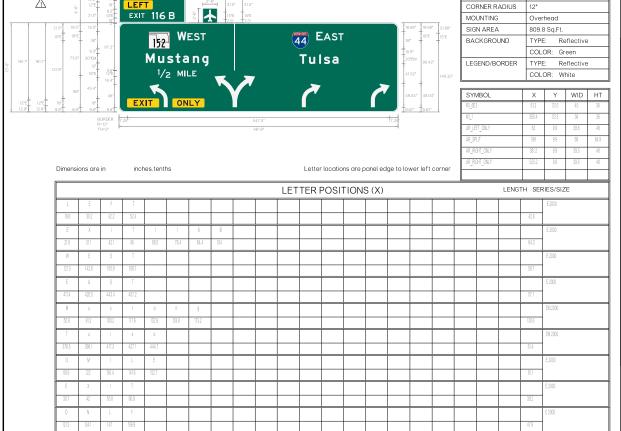
SPECIAL SIGN DETAILS (SHEET 6 OF 14)

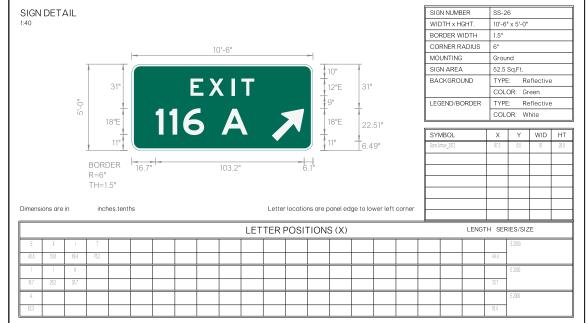
STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECENO. 35154 (04) SHEETINO. T015

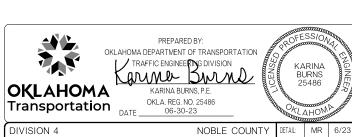










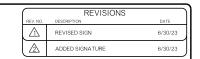


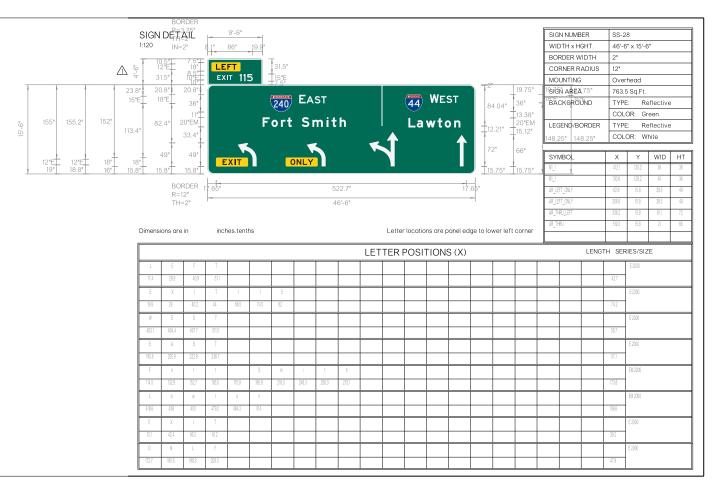
SPECIAL SIGN DETAILS (SHEET 7 OF 14)

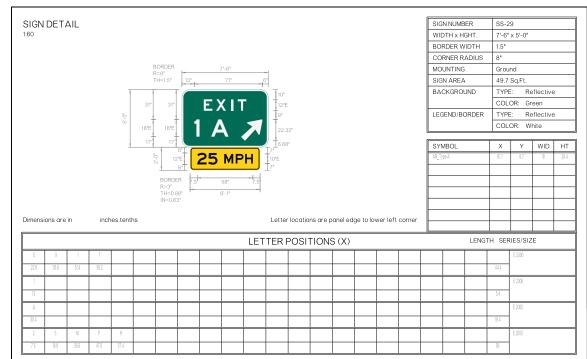
	MILLI 7 OI 147	EM:
	DEPARTMENT OF TRANSP	ORTATION
OKLAHOMA	JOB PIECE NO. 35154 (04)	SHEET NO. TO16

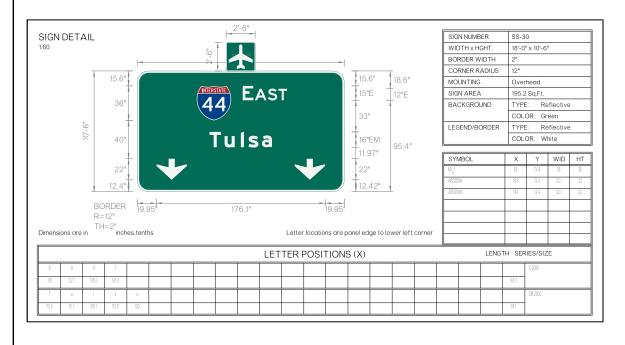
CHECK: KJB 6/23

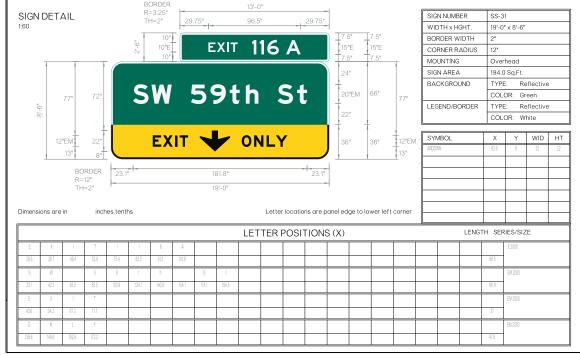
ENGINEER: KJB 6/23











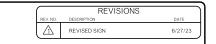


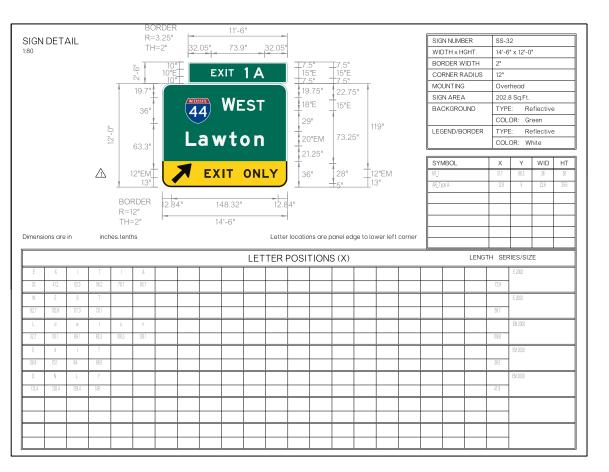
DIVISION 4

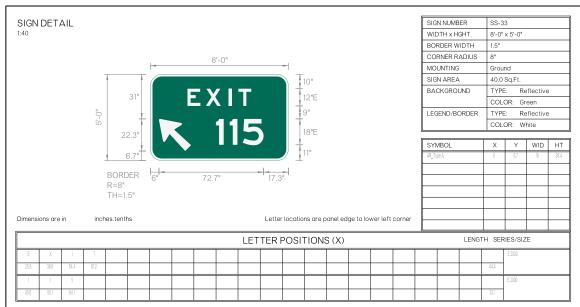
NOBLE COUNTY

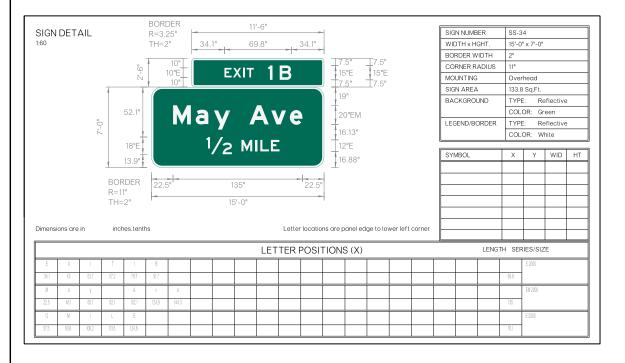
CHECK: KJB 6/23 SPECIAL SIGN DETAILS ENGINEER: KJB 6/23 (SHEET 8 OF 14)

STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECENO. 35154 (04) SHEETNO. T017











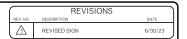


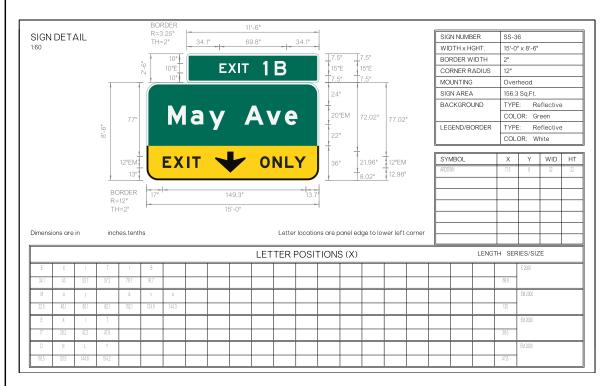
DIVISION 4 NOBLE COUNTY

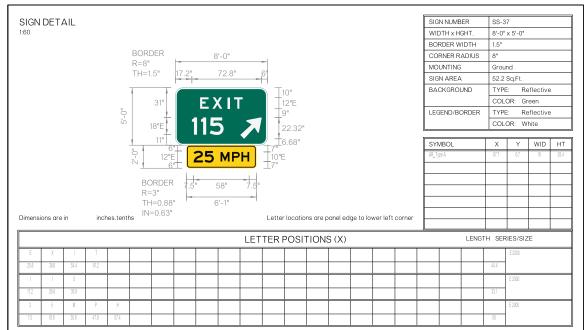
> SPECIAL SIGN DETAILS (SHEET 9 OF

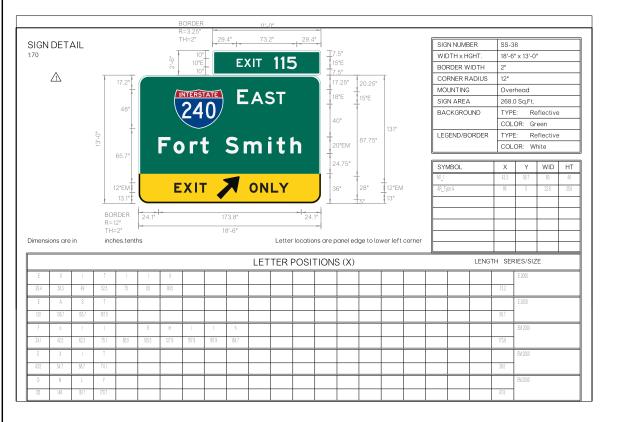
SPECIAL SIGN DETAILS			VID	0/23	
(SHEET 9 OF 14)			GROUP:		
		EM:			
	DEPARTMENT OF TRANSF	ORT	ΔT	ION	
<u> OKLAHOMA </u>	JOB PIECE NO. 35154 (04)		SHEET NO.	T018	

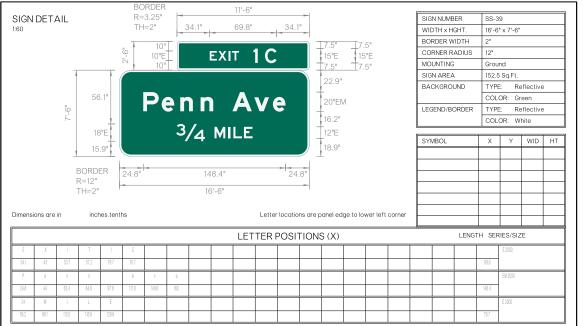
DETAIL: MR 6/23 CHECK: KJB 6/23

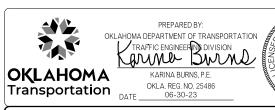












DIVISION 4 NOBLE COUNTY

SPECIAL SIGN DETAILS (SHEET 10 OF 14)

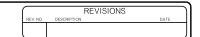
	EM:	
	DEPARTMENT OF TRANSPOR	RTATION
OKLAHOMA	JOB PIECE NO. 35154 (04)	SHEET NO. TO19

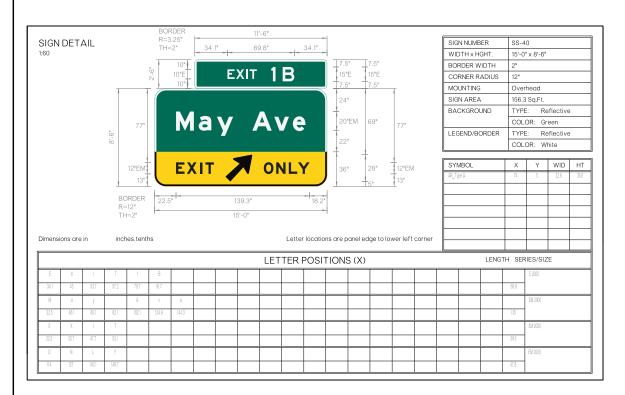
BURNS 25486

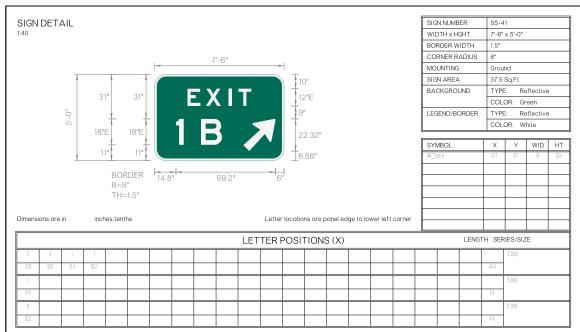
DETAIL: MR 6/23

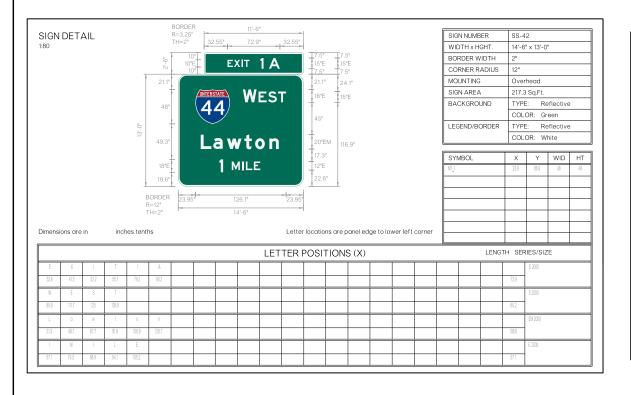
CHECK: KJB 6/23

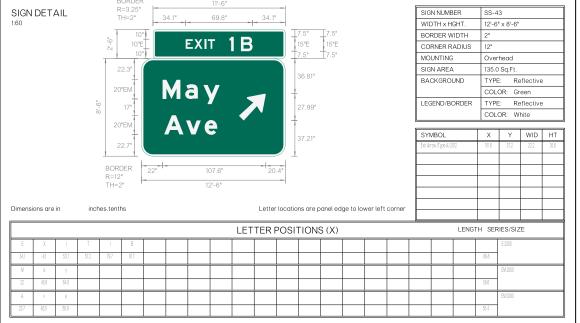
ENGINEER: KJB 6/23

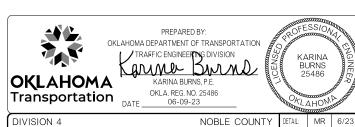












DIVISION 4

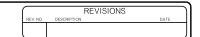
NOBLE COUNTY

CHECK: KJB 6/23

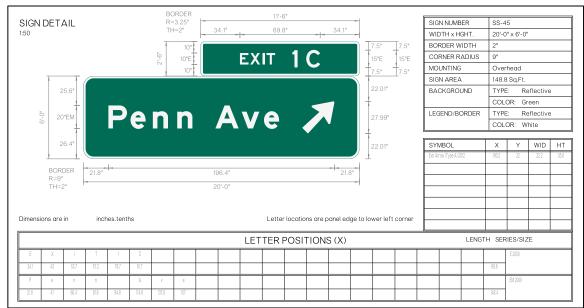
SPECIAL SIGN DETAILS (SHEET 11 OF 14)

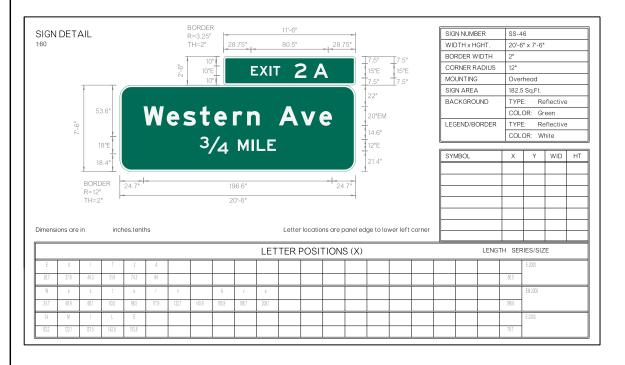
ENGINEER: KJB 6/23

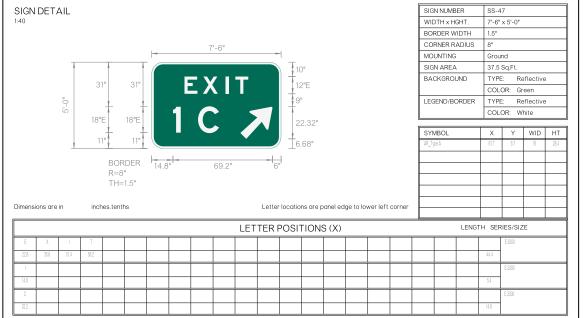
STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECENO. 35154 (04) SHEETNO. T020

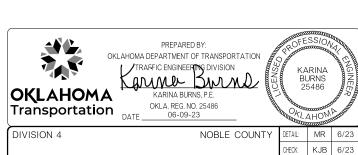












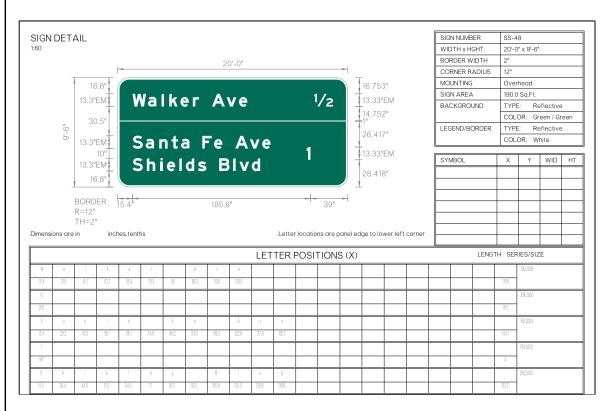
SPECIAL SIGN DETAILS (SHEET 12 OF 14)

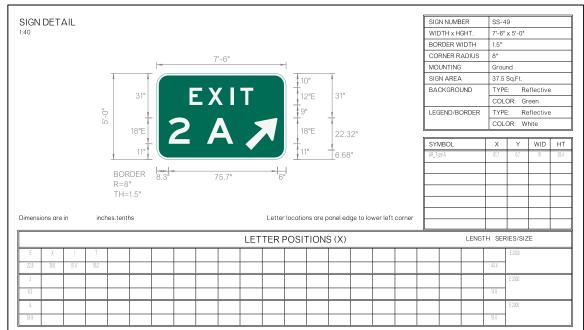
ENGINEER KJB 6/23
GROUP:
EM:

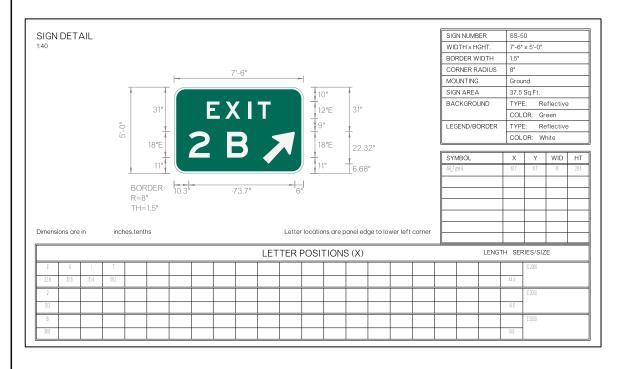
ISPORTATION

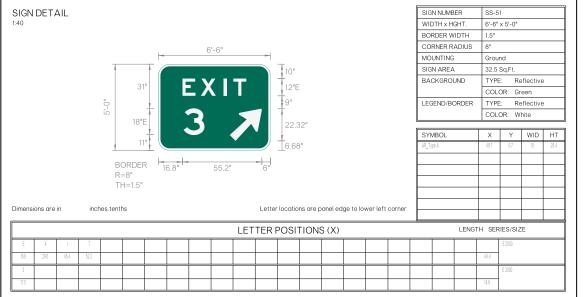
STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECENO. 35154 (04) SHEETNO. T021

REV. NO. DESCRIPTION DATE











DIVISION 4

 NOBLE COUNTY
 DETAIL:
 MR
 6/23

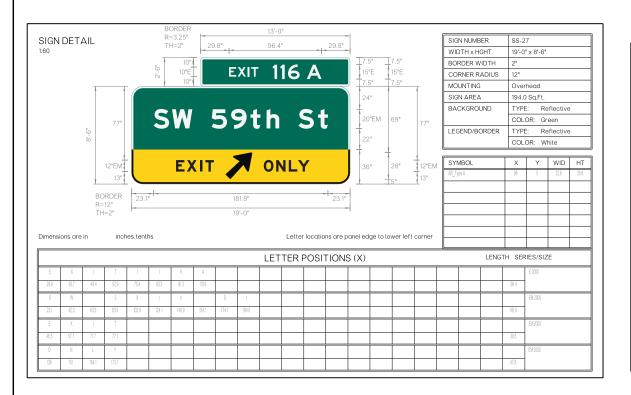
 OHEOK:
 KJB
 6/23

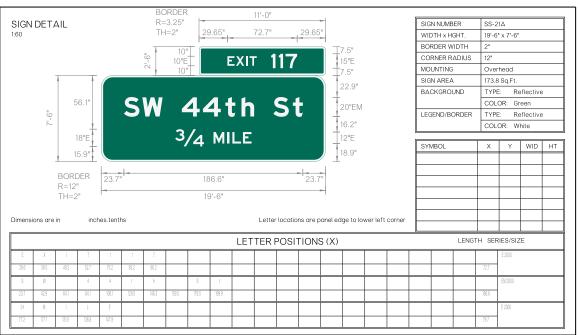
SPECIAL SIGN DETAILS (SHEET 13 OF 14)

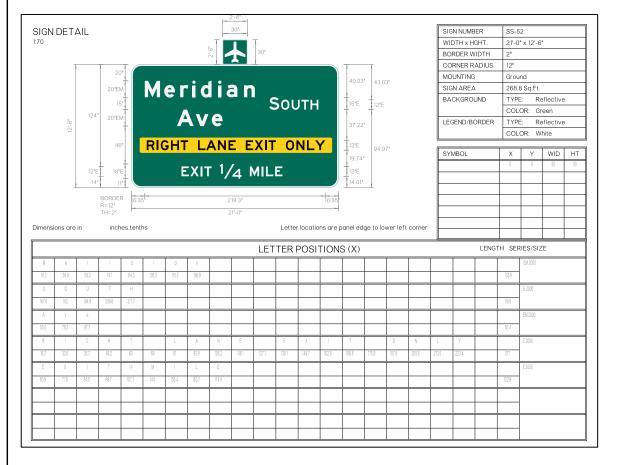
	ENGINEER:	KJB	6/23	
	GROUP:			
	EM:			
NSPORTATION				

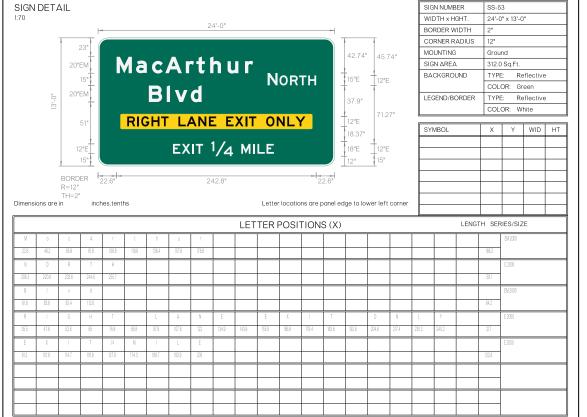
STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECENO. 35154 (04) SHEETINO. TO22

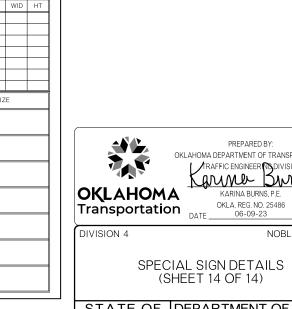
REVISIONS











PREPARED BY: OKLAHOMA DEPARTMENT OF TRANSPORTATION TRAFFIC ENGINEER SOLVISION

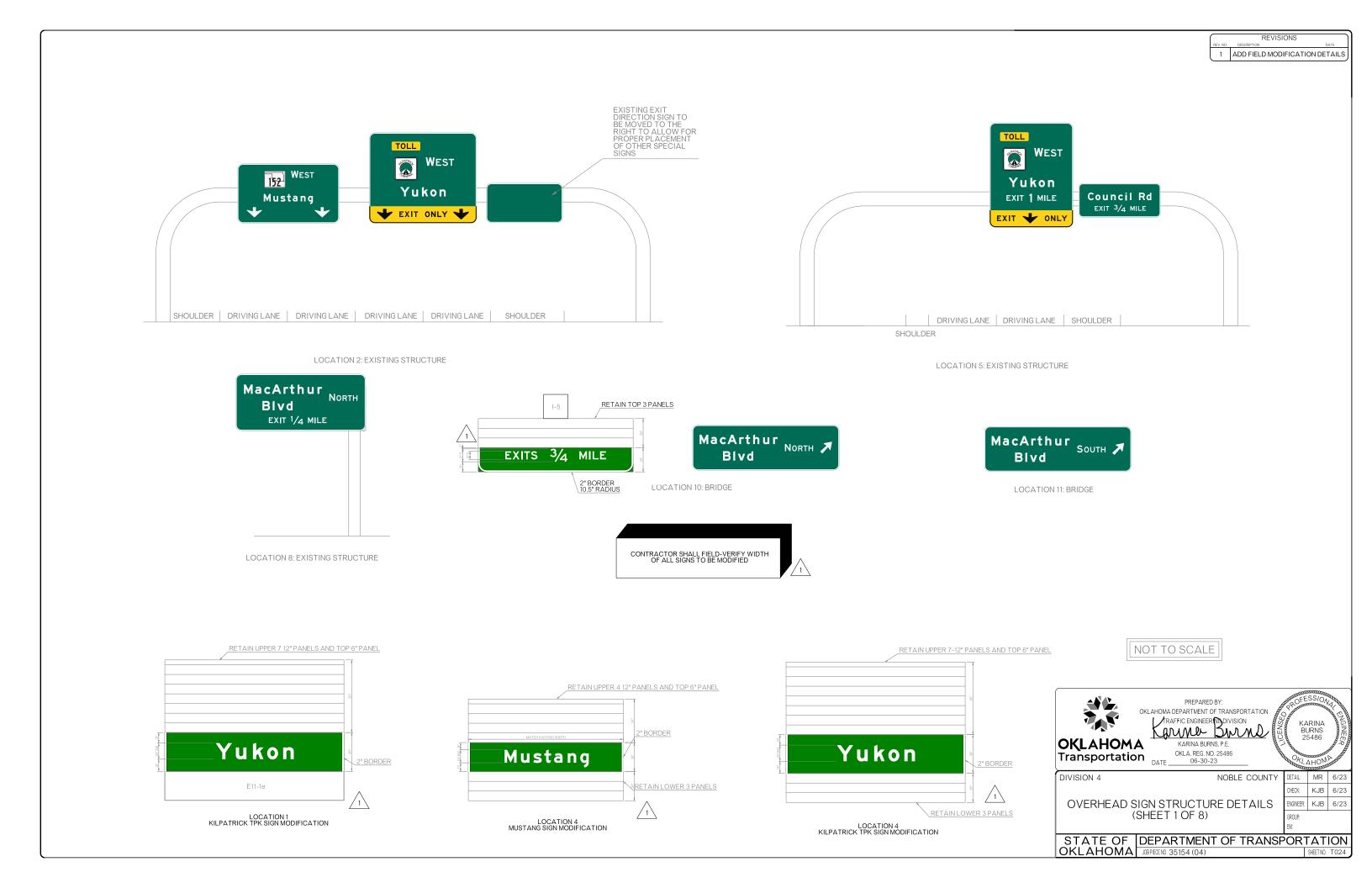
KARINA BURNS, P.E.

NOBLE COUNTY DETAIL: MR 6/23

SPECIAL SIGN DETAILS (SHEET 14 OF 14)

	ENGINEER:	KJB	6/23
	GROUP:		
	EM:		
<u> </u>	ORI	٦Τ	ION

BURNS 25486



$\overline{}$	REVISIONS	
REV. NO.	DESCRIPTION	DATE
Δ	ADDED SIGN	6/29/23
	ADDED NOTE	6/29/23



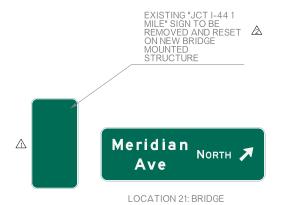
Meridian
Ave
EXIT ONLY

LOCATION 17: EXISTING STRUCTURE

Meridian
Ave
EXIT 1/4 MILE

LOCATION 19: EXISTING STRUCTURE

LOCATION 14: OVERHEAD STRUCTURE 1

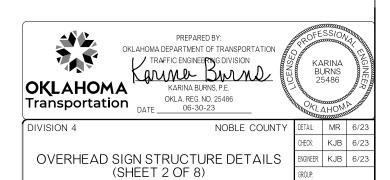


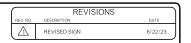




LOCATION 22: BRIDGE

NOT TO SCALE





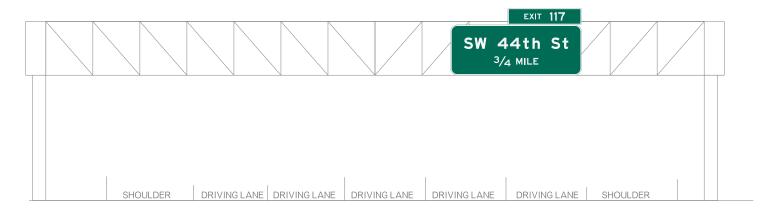


LOCATION 33: OVERHEAD STRUCTURE 2 (100')

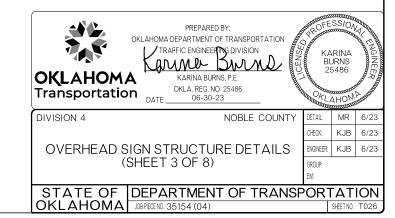
EXISTING SIGN (INCLUDING EXIT TAB AND EXIT ONLY PANELS)
FROM REMOVED STRUCTURE TO BE INSTALLED ON OVERHEAD STRUCTURE 3

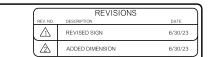
LOCATION 34A: OVERHEAD STRUCTURE 3

| DRIVING LANE | SHOULDER



LOCATION 35: EXISTING STRUCTURE

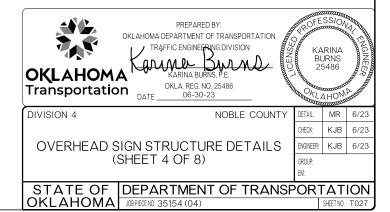






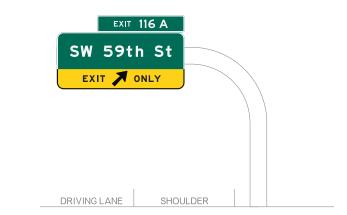


LOCATION 37: OVERHEAD STRUCTURE 5 (95')

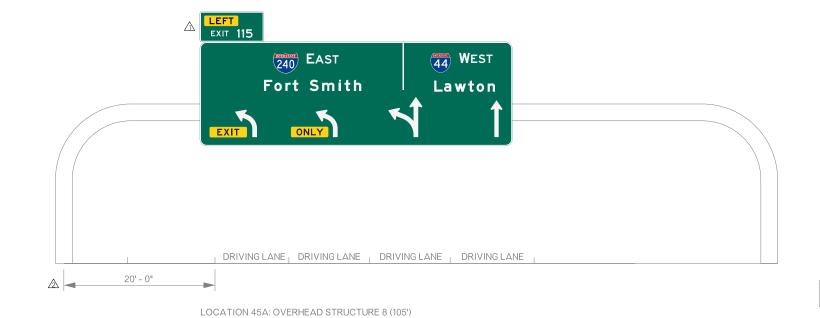


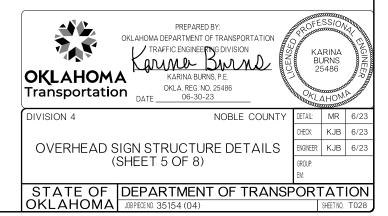
	REVISIONS	
REV. NO.	DESCRIPTION	DATE
$\triangle$	REVISED SIGN	6/22/23
$\bigcirc$	ADDED DIMENSION	6/29/23





LOCATION 44: OVERHEAD STRUCTURE 7





	REVISIONS	
REV. NO.	DESCRIPTION	DATE
$\Box$	REVISED SIGN	6/27/23



May Ave
1/2 MILE

LOCATION 50: BRIDGE

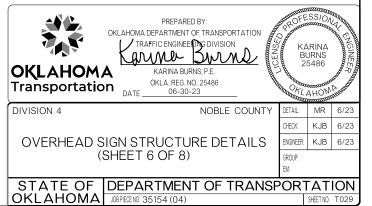
LOCATION 48: OVERHEAD STRUCTURE 9 (100')



LOCATION 52: EXISTING STRUCTURE



LOCATION 54: OVERHEAD STRUCTURE 10



REVISIONS

DESCRIPTION

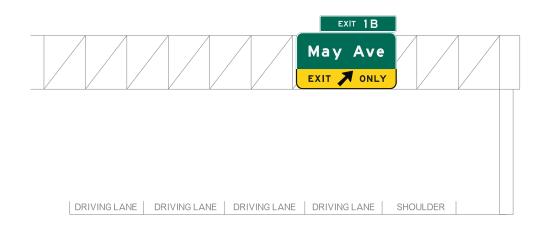
DATE



LOCATION 56: EXISTING STRUCTURE



LOCATION 58: EXISTING STRUCTURE



LOCATION 57: EXISTING STRUCTURE



LOCATION 61: EXISTING STRUCTURE

NOT TO SCALE



DIVISION 4

NOBLE COUNTY DETAIL MR 6/23

NOBLE COUNTY DETAIL MR 6/23

CHECK KJB 6/23

RUCTURE DETAILS

RUCTURE DETAILS

OVERHEAD SIGN STRUCTURE DETAILS (SHEET 7 OF 8)

REVISIONS



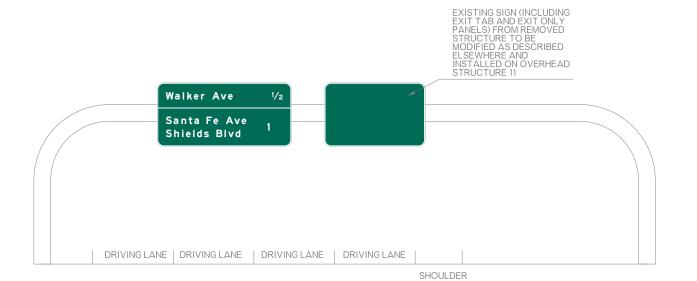
LOCATION 62: OVERHEAD STRUCTURE 12



LOCATION 65: EXISTING STRUCTURE



LOCATION 63: EXISTING STRUCTURE



LOCATION 67: OVERHEAD STRUCTURE 11 (90')

NOT TO SCALE



PREPARED BY:

OKLAHOMA DEPARTMENT OF TRANSPORTATION

TRAFFIC ENGINEER MODIVISION

TRAFFIC ENGINEER MODIVISION KARINA BURNS, P.E.

KARINA BURNS 25486 NOBLE COUNTY

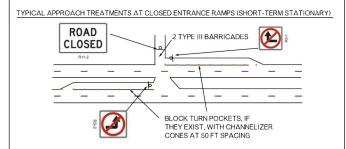
DIVISION 4

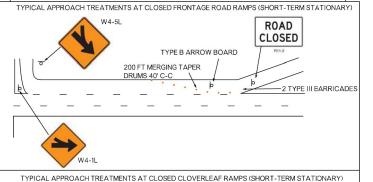
CHECK: KJB 6/23

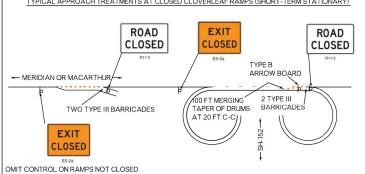
OVERHEAD SIGN STRUCTURE DETAILS (SHEET 8 OF 8)

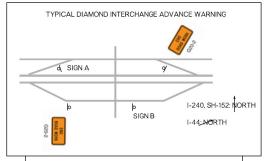
ENGINEER: KJB 6/23

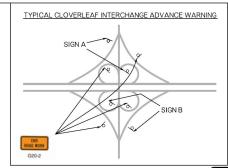
REV.NO. DESCRIPTION DATE













ADVANCED TRAFFIC CONTROL DETAIL

DIVISION 4

STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECE NO. 35154 SHEET NO. T032

NOBLE COUNTY

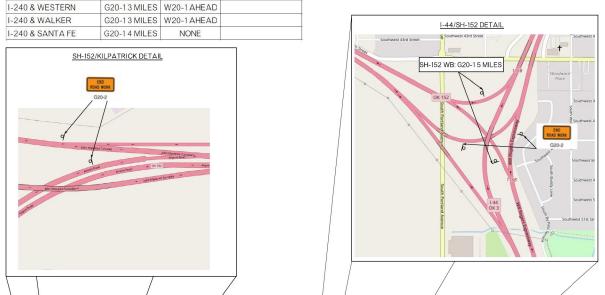
ENGINEER:

PCMS NOTES

IN THE TWO WEEKS LEADING TO THE PROJECT, THE PCMS SHALL READ, ON ONE PAGE: SIGN WORK BEGINS MMM DD WHERE MMM IS THE 3-LETTER MONTH ABBREVIATION AND DD IS THE DAY OF THE MONTH

WHEN WORK IS ACTUALLY UNDERWAY, THE PCMS SHALL READ, ON ONE PAGE: SIGN WORK IN PROGRESS

WHEN THERE IS NO WORK UNDERWAY, THE PCMS SHALL BE IN FLASHING CAUTION MODE, DO NOT USE THE WORD CAUTION ON THE PCMS. SEE 2009 MUTCD FIGURE 6F-6.



INTERCHANGE

SH-152 & MACARTHUR

SH-152 & COUNCIL

SH-152 & MERIDIAN

I-44 & SW 29TH

I-44 & SW 44TH

I-44 & SW 59TH

I-240 & MAY

I-240 & PENN

SIGN A

NONE

G20-13 MILES

G20-12 MILES

G20-13 MILES G20-14 MILES

G20-14 MILES | G20-13 MILES

W20-1 AHEAD | W20-1 AHEAD

W20-1 AHEAD G20-13 MILES

G20-12 MILES | G20-12 MILES

SIGNB

NONE

NONE

NONE

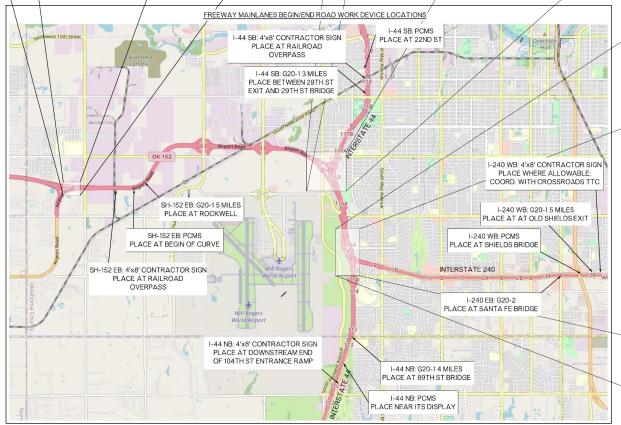
NONE G20-13 MILES OMIT SB G20-2

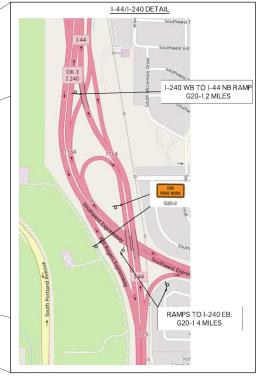
NOTES

OMIT EB G20-2

OMIT BOTH G20-2s

OMIT BOTH G20-2s

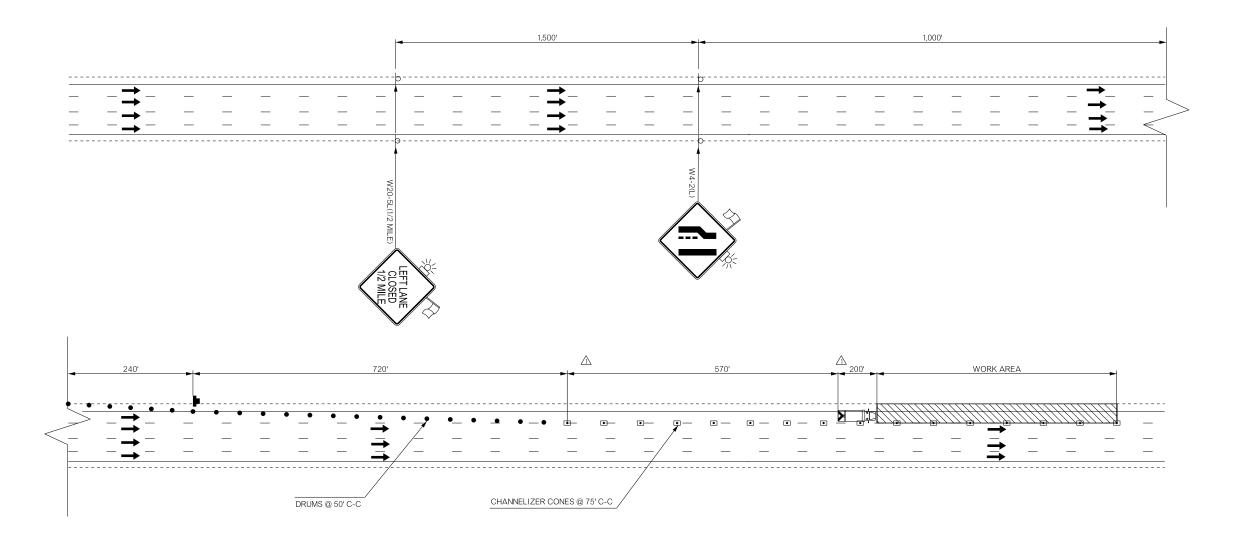


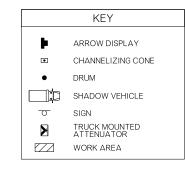


REVISED DETAILS

SHORT TERM STATIONARY

WHERE WORK INCLUDES RECONSTRUCTION OF THE CENTER WALL OF A FREEWAY, TRAFFIC CONTROL SHOWN ON THIS SHEET SHALL BE APPLIED TO BOTH DIRECTIONS OF THE FREEWAY.







PREPARED BY: OKLAHOMA DEPARTMENT OF TRANSPORTATION

LAHOMA DEPARTMENT OF TRANSPORTATION

TRAFFIC ENGINEER OF DIVISION

KARINA BURNS, P.E. OKLA. REG. NO. 25486

DIVISION 4

NOBLE COUNTY

TRAFFIC CONTROL DETAIL SITUATION A-1

CHECK: KJB 6/23 ENGINEER: KJB 6/23

SEE ELSEWHERE IN PLANS FOR WORK TIME RESTRICTIONS

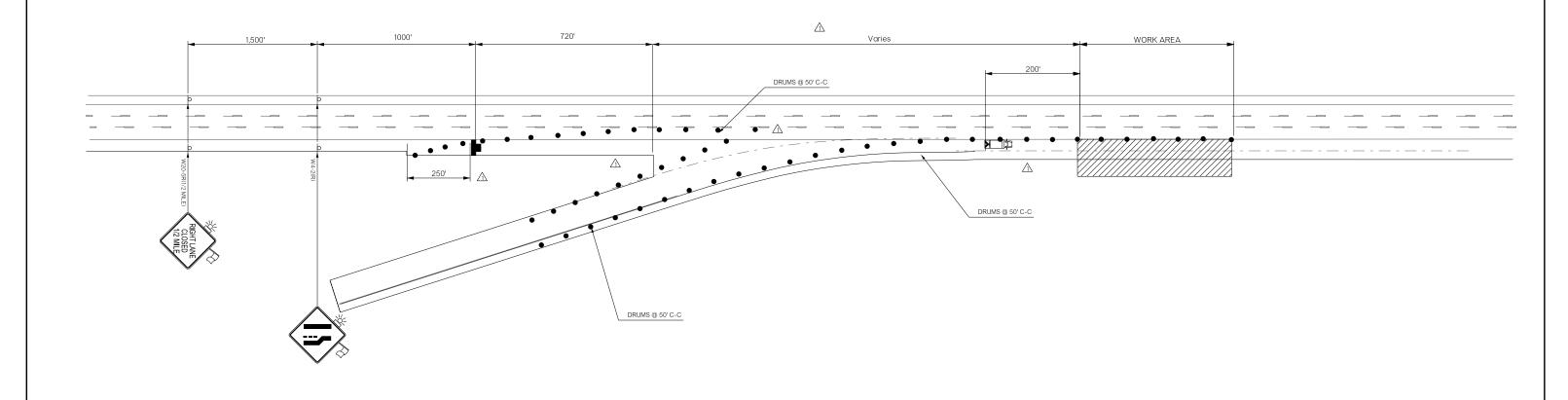
FREEWAY MAY HAVE 3 OR 4 MAINLANES

REVISIONS

BEV. NO. DESCRIPTION DATE

REVISED DETAILS 06/14/23

SHORT TERM STATIONARY



ARROW DISPLAY

CHANNELIZING CONE

DRUM

SHADOW VEHICLE

SIGN
TRUCK MOUNTED
ATTENUATOR
WORK AREA

KEY

FREEWAY MAY HAVE 2 OR 3 LANES UPSTREAM OF THE MERGE DEPICTED HERE

SEE ELSEWHERE IN PLANS FOR WORK TIME RESTRICTIONS

PREPARED BY:
OKLAHOMA DEPARTMENT OF TRANSPORTATION
TRAFFIC ENGINEERING DIVISION
WWW DAVING
KARINA BURNS, P.E.
OKLA, REG. NO. 25486
DATE

Transportation OKLA REG. NO. 25486

DIVISION 4

NOBLE COUNTY

DETAIL JBS 6/23

CHECK KJB 6/23

TRAFFIC CONTROL DETAIL

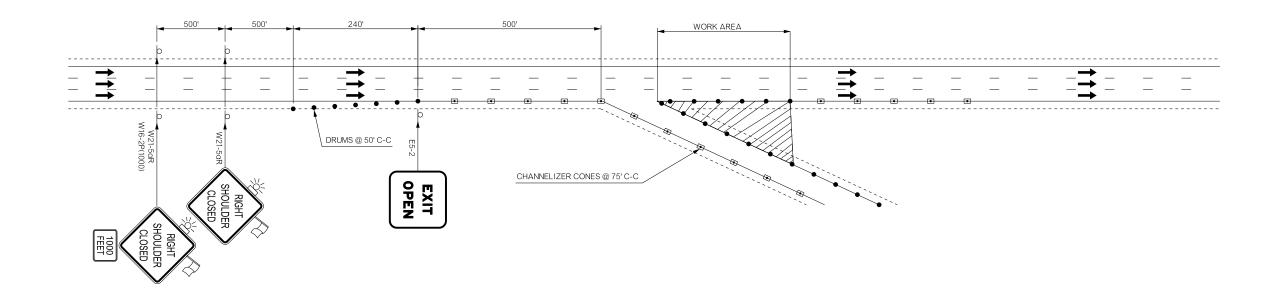
SITUATION A-2

GROUP:
EM.

KARINA BURNS 25486

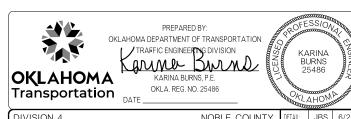
REVISIONS

SHORT TERM STATIONARY



KEY CHANNELIZING CONE DRUM WORK AREA

NOT TO SCALE



DIVISION 4 NOBLE COUNTY

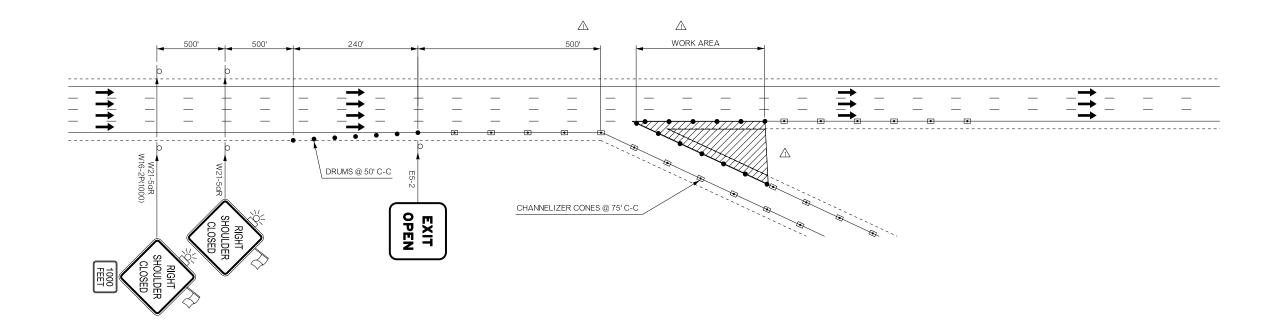
TRAFFIC CONTROL DETAIL SITUATION C-1

ENGINEER: KJB 6/23

SEE ELSEWHERE IN PLANS FOR WORK TIME RESTRICTIONS

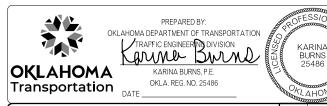
REVISED DETAILS

SHORT TERM STATIONARY



KEY CHANNELIZING CONE DRUM  $\overline{\circ}$ WORK AREA

NOT TO SCALE



DIVISION 4 NOBLE COUNTY

TRAFFIC CONTROL DETAIL SITUATION C-2

STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECENO. 35154 (04) SHEETINO. T036

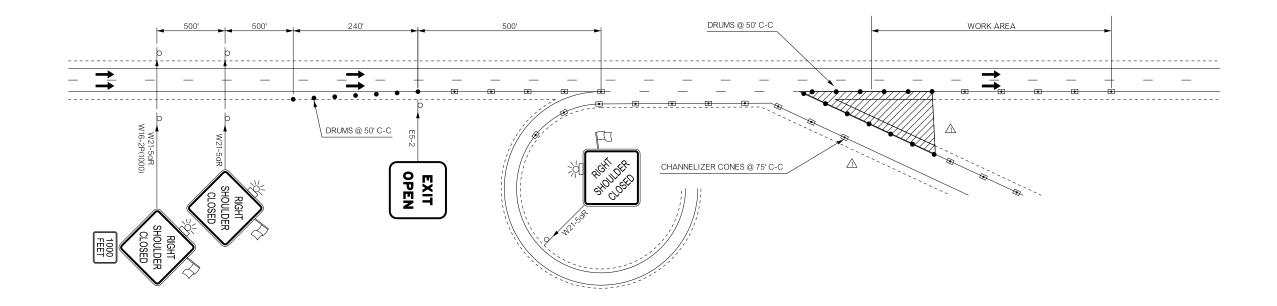
CHECK: KJB 6/23

ENGINEER: KJB 6/23

SEE ELSEWHERE IN PLANS FOR WORK TIME RESTRICTIONS

REVISED DETAILS

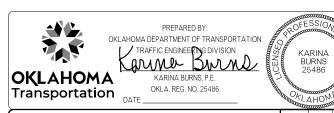
SHORT TERM STATIONARY



KEY CHANNELIZING CONE DRUM 0 WORK AREA

NOT TO SCALE





DIVISION 4 TRAFFIC CONTROL DETAIL SITUATION C-3

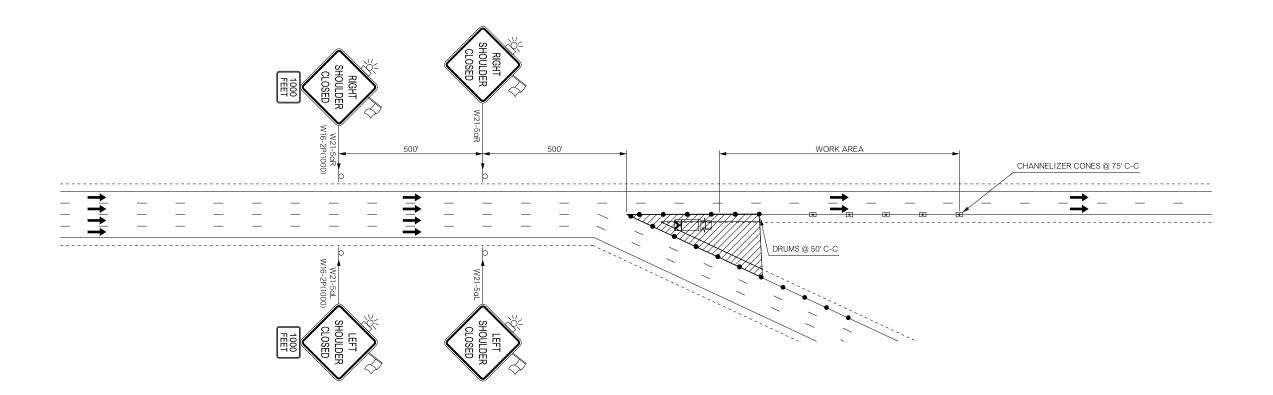
NOBLE COUNTY CHECK: KJB 6/23 ENGINEER: KJB 6/23

STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECENO. 35154 (04) SHEETINO. T037

SEE ELSEWHERE IN PLANS FOR WORK TIME RESTRICTIONS

REV.NO. DESCRIPTION DATE

SHORT TERM STATIONARY



KEY

CHANNELIZING CONE

DRUM

SHADOW VEHICLE

SIGN
TRUCK MOUNTED
ATTENUATOR
WORK AREA



PREFARED BT.

OKLAHOMA DEPARTMENT OF TRANSPORTATION

TRAFFIC ENGINEER DIVISION

WARRINA BURNS, P.E.

OKLA. REG. NO. 25486

DATE

NOBLE COUNTY

DETAIL JBS 6/23

CHECK: KJB 6/23

TRAFFIC CONTROL DETAIL SITUATION C-4

ENGINEER KJB 6/23
GROUP.
EN.
ANSPORTATION

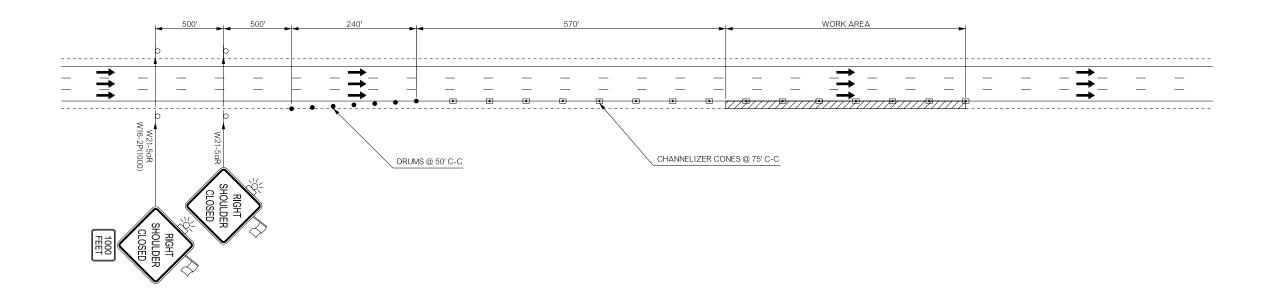
STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECENO. 35154 (04) SHEETINO. T038

SEE ELSEWHERE IN PLANS FOR WORK TIME RESTRICTIONS

FREEWAY MAY HAVE 2 OR 3 LANES ON OTHER SIDE OF THE SPLIT

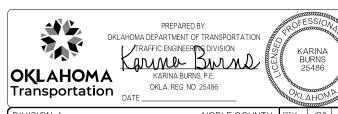
REVISIONS

SHORT TERM STATIONARY



CHANNELIZING CONE WORK AREA

NOT TO SCALE



DIVISION 4 NOBLE COUNTY

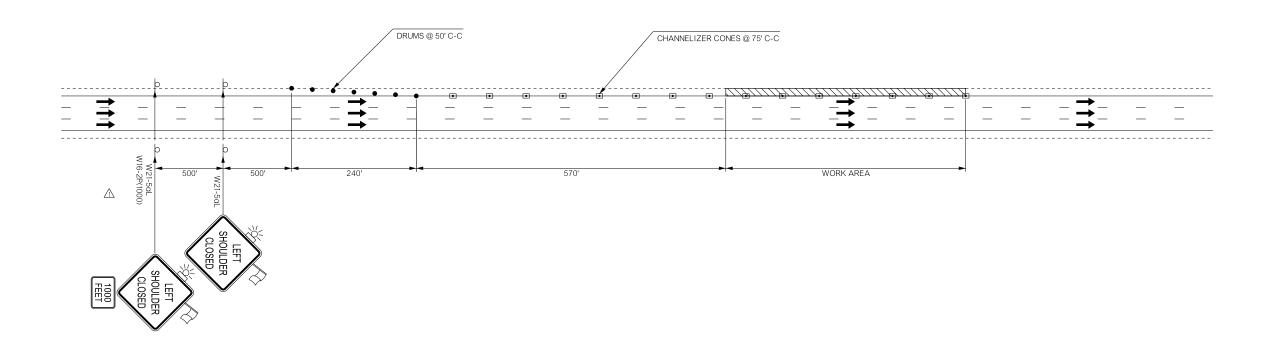
TRAFFIC CONTROL DETAIL SITUATION C-5

ENGINEER: KJB 6/23 STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECENO. 35154 (04) SHEETINO. T039

SEE ELSEWHERE IN PLANS FOR WORK TIME RESTRICTIONS

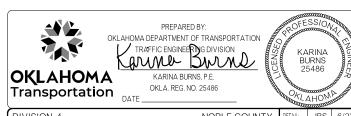
REVISED DETAIL

SHORT TERM STATIONARY



KEY CHANNELIZING CONE DRUM  $\overline{\circ}$ SIGN WORK AREA

NOT TO SCALE



DIVISION 4 TRAF

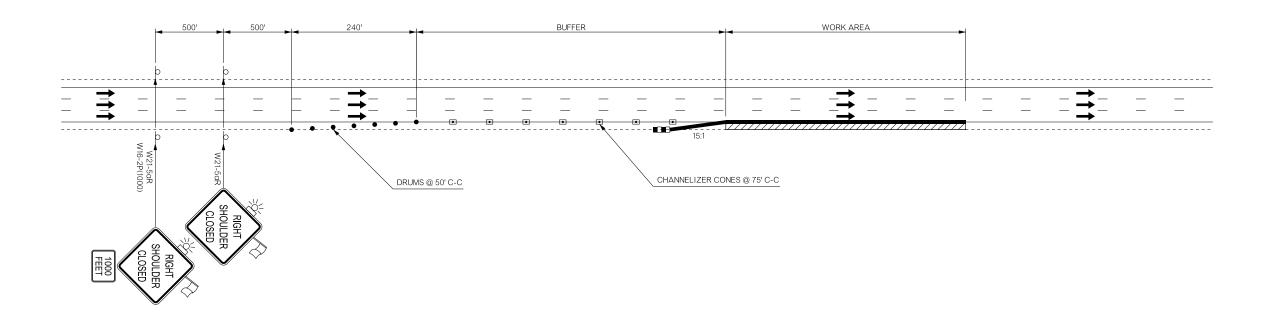
NOBLE COUN	ΙY	DETAIL:	JR2	6/23
		CHECK:	KJB	6/23
FFIC CONTROL DETAIL		ENGINEER:	KJB	6/23
SITUATION C-6		GROUP:		
		EM:		

STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECENO. 35154 (04) SHEETINO. TO40

SEE ELSEWHERE IN PLANS FOR WORK TIME RESTRICTIONS

REVISIONS

LONG TERM STATIONARY



KEY CHANNELIZING CONE CONSTRUCTION ZONE IMPACT ATTENUATOR PORTABLE LONGITUDINAL BARRIER WORK AREA

NOT TO SCALE



KLA. REG. NO.	25486 <b>%</b>	MINION OKL	accountered AHOM annuante	A STEELING STATE OF THE STATE O
	NOBLE COUNTY	DETAIL:	JBS	6/23
		CHECK:	KJB	6/23
ROL DE	TAIL	ENGINEER:	KJB	6/23
N C-7		GROUP:		
		EM:		

KARINA BURNS 25486

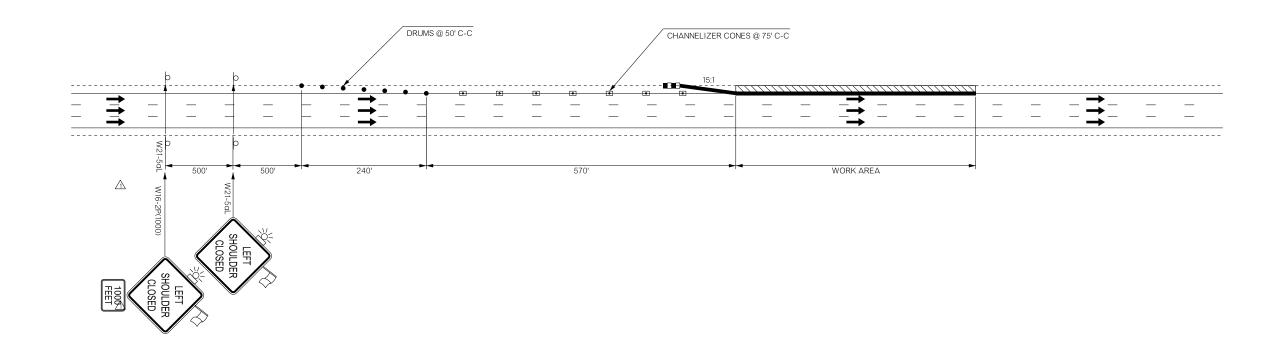
REVISIONS

REV. NO DESCRIPTION DATE

REVISED DETAIL 6/29/23

LONG TERM STATIONARY

WHERE WORK INCLUDES RECONSTRUCTION OF THE CENTER WALL OF A FREEWAY, TRAFFIC CONTROL SHOWN ON THIS SHEET SHALL BE APPLIED TO BOTH DIRECTIONS OF THE FREEWAY.



CONSTRUCTION ZONE
 IMPACT ATTENUATOR
 DRUM
 PORTABLE
 LONGITUDINAL BARRIER

TO SUCK.

CHANNELIZING CONE

KEY

o sign

WORK AREA

OKLAHOMA DEPA TRAFFIC JW OKLAHOMA K, OK Transportation

PREPARED BY:
OKLAHOMA DEPARTMENT OF TRANSPORTATION
TRAFFIC ENGINEER OF DIVISION
KARINA BURNS, P.E.
OKLA. REG. NO. 25486

RANSPORTATION DIVISION KARINA BURNS 25486

DIVISION 4

NOBLE COUNTY

TRAFFIC CONTROL DETAIL SITUATION C-8

CHECK: KJB 6/23

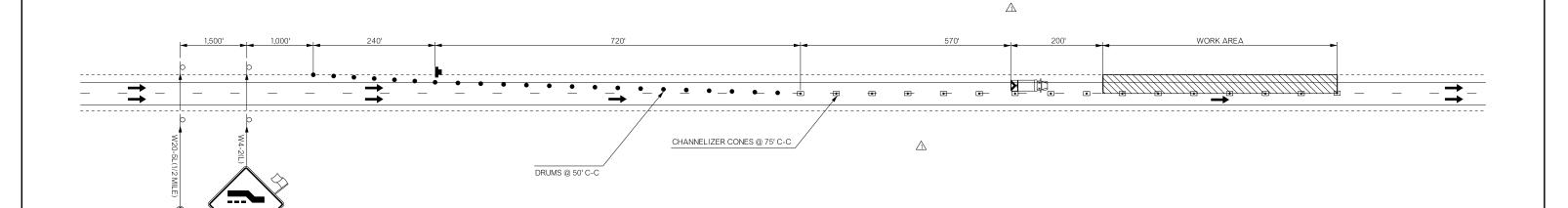
ENGINEER: KJB 6/23

GROUP:
EM:

STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECENO. 35154 (04) SHEETINO. T042

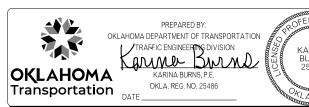
REVISED DETAIL

SHORT TERM STATIONARY



ARROW DISPLAY CHANNELIZING CONE SHADOW VEHICLE AAA TYPE III BARRICADE WORK AREA

NOT TO SCALE



TRAFFIC CONTROL DETAIL SITUATION D-1

STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECENO. 35154 (04) SHEETINO. T043

CHECK: KJB 6/23

ENGINEER: KJB 6/23

SEE ELSEWHERE IN PLANS FOR WORK TIME RESTRICTIONS

REVISIONS REVISED DETAILS 06/14/23

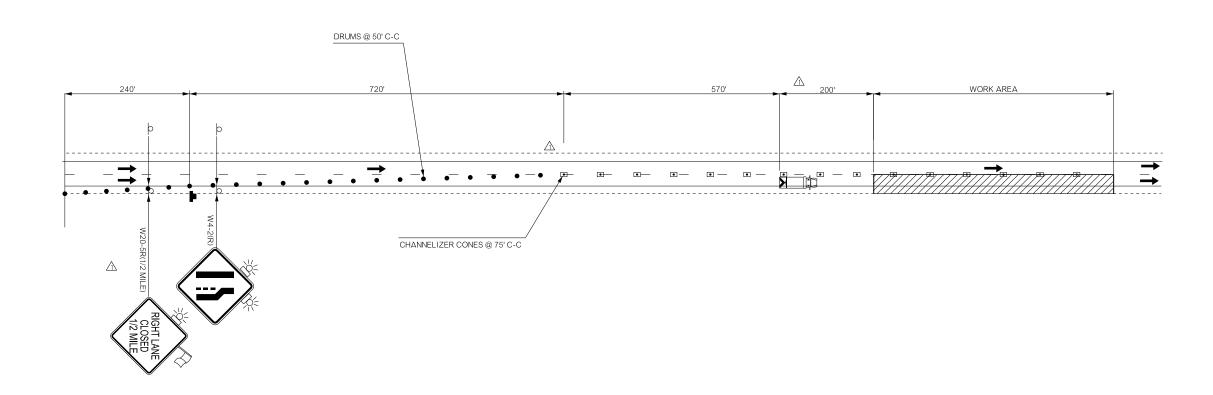
Δ

NOT TO SCALE

SHORT TERM STATIONARY

Entrance ramps, or their acceleration lanes, that connect to the freeway within the traffic control area shall be closed for the duration of work. the plans for ramp closure details.

Exit ramps, or their deceleration lanes, that connect to the freeway within the traffic control area shall be closed for the duration of work. Place one E5-2a (EXIT CLOSED) 500 ft upstream of the exit and another E5-2a 1000 ft upstream of the exit.



ARROW DISPLAY CHANNELIZING CONE PORTABLE CHANGEABLE MESSAGE SIGN SHADOW VEHICLE  $\overline{\circ}$ SIGN TRUCK MOUNTED ATTENUATOR AAA TYPE III BARRICADE WORK AREA



PREPARED BY: OKLAHOMA DEPARTMENT OF TRANSPORTATION TRAFFIC ENGINEERING DIVISION Karina Burns, P.E. OKLA. REG. NO. 25486

DIVISION 4 NOBLE COUNTY

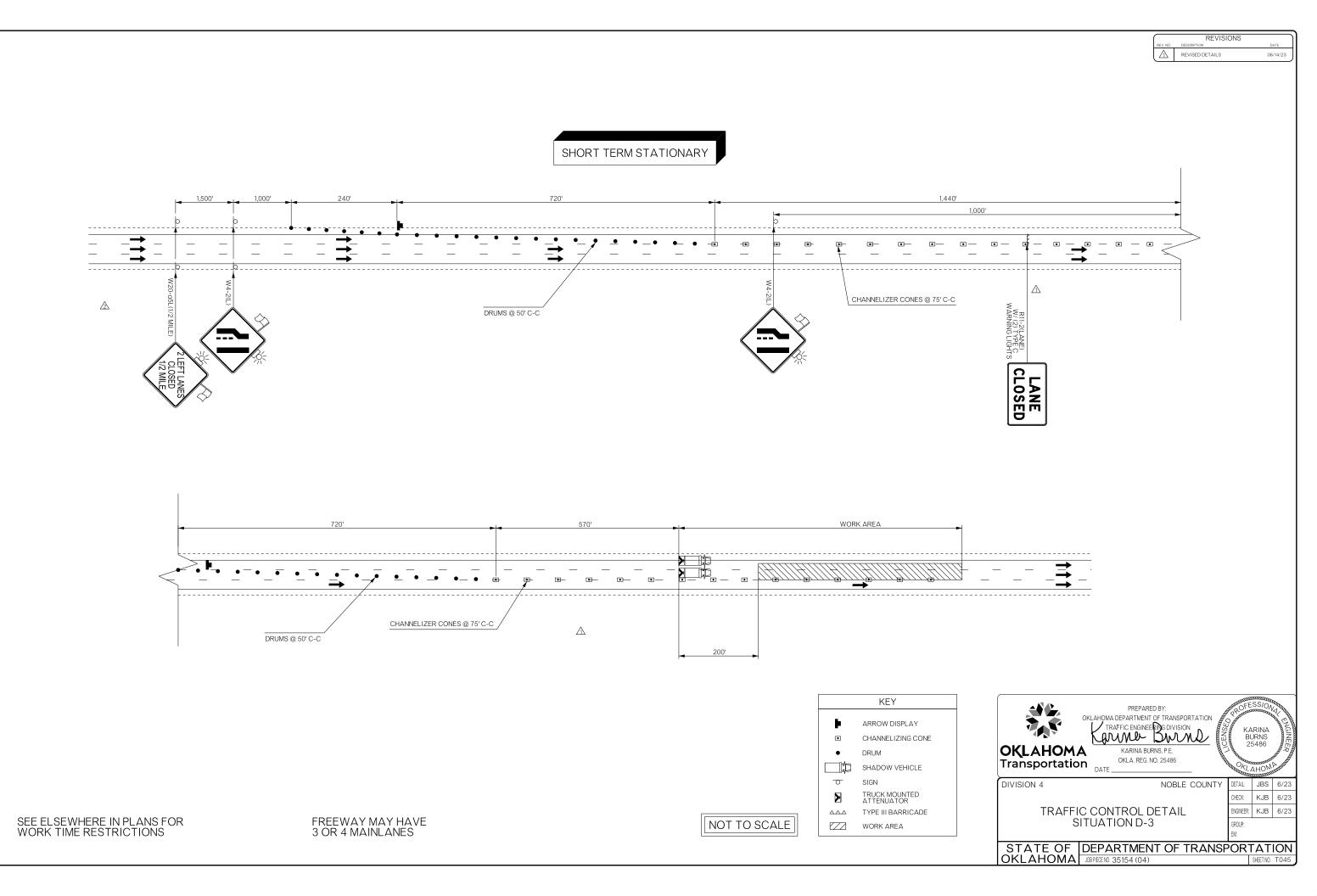
> TRAFFIC CONTROL DETAIL SITUATION D-2

ENGINEER: KJB 6/23

SEE ELSEWHERE IN PLANS FOR WORK TIME RESTRICTIONS

STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECENO. 35154 (04) SHEETNO. T044

CHECK: KJB 6/23

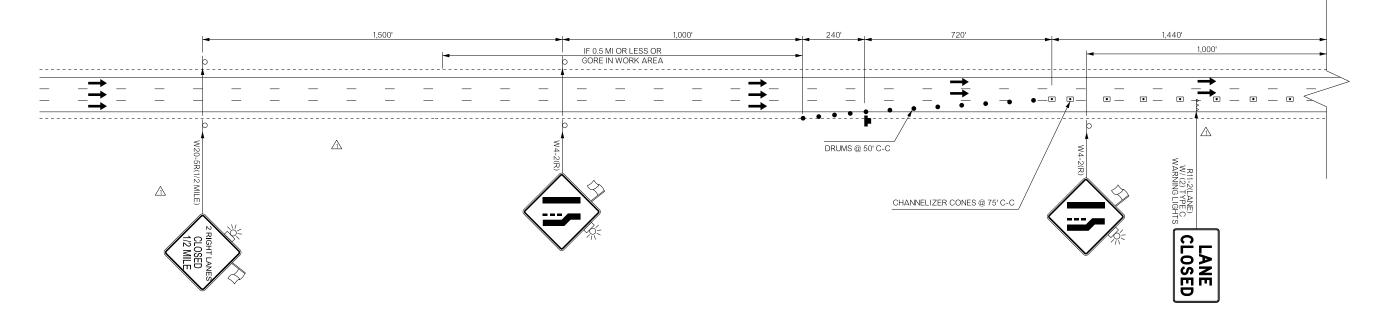


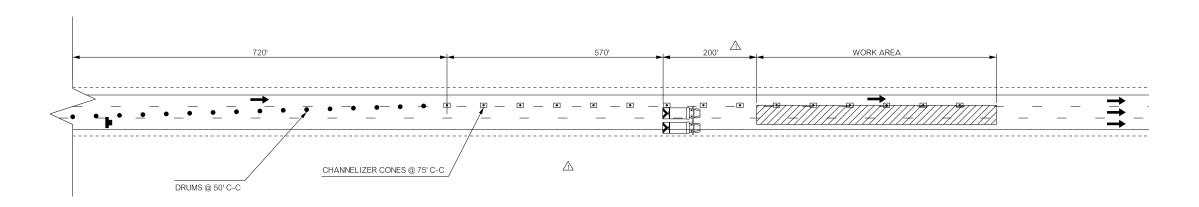
REVISED DETAILS 06/15/23

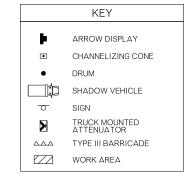
ENTRANCE RAMPS, OR THEIR ACCELERATION LANES, THAT CONNECT TO THE FREEWAY WITHIN THE TRAFFIC CONTROL AREA SHALL BE CLOSED FOR THE DURATION OF WORK.
SEE ELSEWHERE IN THE PLANS FOR RAMP CLOSURE DETAILS.

EXIT RAMPS, OR THEIR DECELERATION LANES,
THAT CONNECT TO THE FREEWAY WITHIN
THE TRAFFIC CONTROL AREA SHALL BE CLOSED FOR THE
DURATION OF WORK.
PLACE ONE E5-2A (EXIT CLOSED)
500 FT UPSTREAM OF THE EXIT AND ANOTHER E5-2A 1000 FT
UPSTREAM OF THE EXIT.

SHORT TERM STATIONARY









TRAFFIC CONTROL DETAIL SITUATION D-4

STATE OF DEPARTMENT OF TRANSPORTATION OKLAHOMA JOBPIECENO. 35154 (04) SHEETNO. T046

CHECK: KJB 6/23

ENGINEER: KJB 6/23

SEE ELSEWHERE IN PLANS FOR WORK TIME RESTRICTIONS

FREEWAY MAY HAVE 3 OR 4 MAINLANES

													Δ												
								A		3	<u> </u>	SIG	N NUME	BER	<u> </u>	<u> </u>			<u> </u>		<u> </u>		ß	<u> </u>	
WORK TYPE	1	2	3	4	5	6	7	8	9	10	11	12	12 A	13	13 A	14	15	16	17	18	19	20	21	22	23
FOOTING PLACEMENT - RIGHT						C-5*	C-5*		C-3*			C-3*	C-5*	C-2*		E-14	C-5*	C-5*		C-2*		C-3*			C-3*
FOOTING CURING - RIGHT						NONE	NONE		NONE			NONE	NONE	NONE		C-7	NONE	NONE		NONE		NONE			NONE
POST/STRUCTURE PLACEMENT - RIGHT		C-1				C-5*	C-5*		C-3*			C-3*	C-5*	C-2*		E-14	C-5*	C-5*		C-2*		C-3*			C-3*
STRUCTURE PLACEMENT - OVERHEAD																D-4*									
SIGN REMOVAL		C-1	TB*	E-3	TB*			D-2*	C-3*	D-1* D-2*	D-2*	C-3*		C-2*	C-5*		C-5*	C-5*	E-17	C-2*	E-17	C-3*	D-1* D-2*	D-1* D-2*	C-3*
SIGN PLACEMENT OR MODIFICATION	E-1	C-1	E-1	E-3	D-1* D-2*	C-5*	C-5*	D-2*	C-3*	D-1* D-2*	D-2*	C-3*	C-5*	C-2*		D-4*	C-5*	C-5*	E-17	C-2*	E-17	C-3*	D-1* D-2*	D-1* D-2*	C-3*
													<u> </u>												
												A 010h	<u> </u>	)ED ^		^	^	^							_

	REVISIONS	
REV. NO.	DESCRIPTION	DATE
Δ	REVISED ROW NAME	6/29/23
$\triangle$	ADDED TEXT	6/29/23
<u> </u>	REVISED COLUMN VALUES	6/29/23

					<u> </u>		A					∆ SIGI	N NÜME	BER∆		<u> </u>	<u> </u>	<u> </u>							
WORK TYPE	24	25	26	27	27 A	28	28 A	29	30	31	32	33	34	34 A	35	36	36 A	37	38	39	40	40 A	41	42	43
FOOTING PLACEMENT - LEFT				C-6*					C-6*			Δ-1*		Δ-1*		Δ-1*		Δ-1*			Δ-1*				
FOOTING CURING - LEFT				NONE					NONE			C-8		C-8		C-8		C-8			C-8				
FOOTING PLACEMENT - RIGHT	C-1 *	C-5*	C-5*			C-5*				C-5*	C-4	CP1R*		CP1R*		CP1R*		CP1R*	C-2		A-2*		C-2*		C-1*
FOOTING CURING - RIGHT	NONE	NONE	NONE			NONE				NONE	NONE	C-7		C-7		C-7		C-7	NONE		E-40		NONE		NON
POST/STRUCTURE PLACE/REM - LT				C-6*					C-6*			Δ-1*		Δ-1*		Δ-1*	A-1*	Δ-1*			A-1*	A-1*			
POST/STRUCTURE PLACE/REM - RT	C-1 *	C-5*	C-5*			C-5*				C-5*	C-4	CP1R*	CP1R*	CP1R*		CP1R*	CP1R*	CP1R*	C-2		A-2*	A-2*	C-2*		C-1*
STRUCTURE PLACE/REM - OVERHEAD												TB*	CP1R*	TB		TB*	TB*	TB*			TB*	TB*			
SIGN REMOVAL	C-1 *	C-5*	C-5*		C-6*		C-5*	C-5*	C-6*	C-5*	C-4				D-3* D-4*				C-2	C-6			C-2*	C-6*	C-1*
SIGN PLACEMENT OR MODIFICATION	C-1 *	C-5*	C-5*	C-6*		C-5*			C-6*	C-5*	C-4	CP2L* D-4*		D-3* CP2R*	D-4*	D-3* D-4*		E-37B E-37C	(:=')		D-3* D-4*		C-2*		C-1*

	<u> </u>	<u> </u>				<u> </u>	<u> </u>	<u> </u>		<u> </u>		SIGI	N NUME	3ER	<u> </u>						<u> </u>		<u> </u>		
WORK TYPE	44	44 A	45	45 A	46	47	47 A	48	49	50	51	52	53	54	55	56	57	58	59	59 A	60	61	62	63	64
FOOTING PLACEMENT - LEFT				Δ-1*				Δ-1*																	
FOOTING CURING - LEFT				C-8				C-8																	
FOOTING PLACEMENT - RIGHT	CP1R			A-2*		C-2	C-5	CP1R	C-4*					A-2	C-2*				C-2	C-2			A-2		
FOOTING CURING - RIGHT	C-7			C-7		NONE	NONE	C-7	NONE					C-7	NONE				NONE	NONE			C-7		
POST/STRUCTURE PLACE/REM - LT				Δ-1*	Δ-1*			Δ-1*																	
POST/STRUCTURE PLACE/REM - RT	CP1R	CP1R		A-2*	A-2*	C-2	C-5	A-2*	C-4*					A-2	C-2*				C-2	C-2			A-2		
STRUCTURE PLACE/REM - OVERHEAD	CP1R	CP1R		TB*	TB*			TB*		CP2R*				A-2									A-2		
SIGN REMOVAL			C-5			C-2	C-5		C-4*	CP2R*	C-5	D-4*	C-5	A-2	C-2*	D-1*	D-3*	D-3*	C-2		C-2	D-3*		D-4*	C-1
SIGN REIVIOVAL			C-5			0-2	U-5		C-4	CFZR	C-5	D-4	C-5	A-2	U-2	D-2*	D-4*	D-4*	U-2		0-2	D-4*		D-4	C-1
SIGN PLACEMENT OR MODIFICATION	CP1R			D-3* E-45A		C-2	C-5	D-3* CP2R*	C-4*	CP2R*		D-4*		A-2	C-2*	D-1* D-2*	D-3* D-4*	D-3* D-4*	C-2	C-2		D-3* D-4*	D-4*	D-4*	

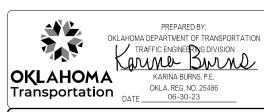
							<u>/3\</u>							
			ß			<u> </u>	SIGN 1	NUMBER		<u> </u>	ß			<u> </u>
WORK TYPE	65	66	67	68	69	70	71	72	73	74	75	76	79	80
FOOTING PLACEMENT - LEFT			Δ-1*											
FOOTING CURING - LEFT			C-8											
FOOTING PLACEMENT - RIGHT		C-1	A-2*	C-2	C-2			C-2	C-2			C-2	C-2	
FOOTING CURING - RIGHT		NONE	C-7	NONE	NONE			NONE	NONE			NONE	NONE	
POST/STRUCTURE PLACE/REM - LT			Δ-1*											
POST/STRUCTURE PLACE/REM - RT		C-1	A-2*	C-2	C-2			C-2	C-2			C-2	C-2	
STRUCTURE PLACE/REM - OVERHEAD			TB*											
SIGN REMOVAL	D-4*	C-1		C-2	C-2			C-2	C-2			C-2	C-2	
SIGN PLACEMENT OR MODIFICATION	D-4*	C-1	D-3* E-67	C-2	C-2	D-4*	D-4*	C-2	C-2	D-4*	D-4*	C-2	C-2	D-4*

\* SUBJECT TO WORK TIME RESTRICTIONS

TB = TRAFFIC BREAK

△ CP1R: RIGHT LANE CLOSED, SHORT TERM STATIONARY CP2R: 2 RIGHT LANES CLOSED, SHORT TERM STATIONARY CP2L: 2 LEFT LANES CLOSED, SHORT TERM STATIONARY

△ SETUPS CP1R, CP2R, CP2L: CONTRACTOR SHALL SUBMIT TTC PLANS FOR APPROVAL TO ODOT CENTRAL OFFICE TRAFFIC DIVISION TWO WEEKS BEFORE WORK IS TO BEGIN ON THESE SETUPS.



OKLAHOMA DEPARTMENT OF TRANSPORTATION

NOBLE COUNTY

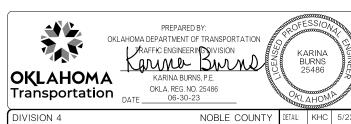
TEMPORARY TRAFFIC CONTROL SET-UPS | NONEER | KJB | 6/23

_	REVISIONS	
REV. NO.	DESCRIPTION	DATE
Δ	ADDED ROW	6/29/23

SIGN	SETUP	RESTRICTION		
1	D-4	NO WORK 07:00-09:00 MON-FRI, 12:00-19:00 MON-FRI		
3	ТВ	00:00 TO 04:00 ONLY		
5	TB	00:00 TO 04:00 ONLY		
5	D-1 & D-2	23:00 TO 05:00 ONLY, EXCEPT WORK MAY CONTINUE TO 08:00 SAT OR 09:00 SUN		
6	C-5	NO WORK 06:00-09:00 MON-FRI, 12:00-19:00 MON-FRI, 12:00-17:00 SAT		
7	C-5	NO WORK 06:00-09:00 MON-FRI, 12:00-19:00 MON-FRI, 12:00-17:00 SAT		
8	D-2	23:00 TO 05:00 ONLY, EXCEPT WORK MAY CONTINUE TO 07:00 SAT OR 09:00 SUN		
9	C-3	NO WORK 07:00-09:00 MON-FRI, 14:00-19:00 MON-FRI		
10		23:00 TO 05:00 ONLY, EXCEPT WORK MAY CONTINUE TO 07:00 SAT OR 08:00 SUN		
11	D-2	23:00 TO 05:00 ONLY, EXCEPT WORK MAY CONTINUE TO 07:00 SAT OR 09:00 SUN		
12	C-3	NO WORK 06:00-09:00 MON-FRI, 12:00-19:00 MON-FRI, 12:00-17:00 SAT		
12 A	NO WORK 07:00-09:00 MON-FRI, 14:00-19:00 MON-FRI			
13	C-5 C-2	NO WORK 07:00-09:00 MON-FRI, 14:00-19:00 MON-FRI		
13 A	NO WORK 07:00-09:00 MON-FRI, 14:00-19:00 MON-FRI			
14	C-5 C-5	NO WORK 07:00-09:00 MON-FRI, 14:00-19:00 MON-FRI		
14	D-4	23:00 TO 05:00 ONLY		
		NO WORK 06:00-09:00 MON-FRI, 12:00-19:00 MON-FRI, 12:00-17:00 SAT		
· · · · · · · · · · · · · · · · · · ·		NO WORK 07:00-09:00 MON-FRI, 14:00-19:00 MON-FRI		
		NO WORK 06:00-09:00 MON-FRI, 12:00-19:00 MON-FRI, 12:00-17:00 SAT		
20 C-3 NO WORK 07:00-09:00 MON-FRI, 14:00-19:00 MON-FRI				
21 D-1 & D-2   23:00 TO 05:00 ONLY, EXCEPT WORK MAY CONTINUE TO 07:00 SAT OR 0				
22 D-1 & D-2 23:00 TO 05:00 ONLY, EXCEPT WORK MAY CONTINUE TO 07:00 SAT OR 05:				
22 D-1 & D-2 23.00 TO 05.00 ONLY, EXCEPT WORK MAY CONTINUE TO 07.00 SAT 23 C-3 NO WORK 06:00-09:00 MON-FRI, 12:00-19:00 MON-FRI, 12:00-17:00 SA				
24	C-1	NO WORK 07:00-09:00 MON-FRI, 14:00-19:00 MON-FRI		
25	C-5	NO WORK 07:00-09:00 MON-FRI, 14:00-19:00 MON-FRI		
26	C-5	NO WORK 07:00-09:00 MON-FRI, 14:00-19:00 MON-FRI		
27	C-6	NO WORK 06:00-09:00 MON-FRI, 12:00-19:00 MON-FRI, 12:00-17:00 SAT		
27 A		NO WORK 06:00-09:00 MON-FRI, 12:00-19:00 MON-FRI, 12:00-17:00 SAT		
28	C-5	NO WORK 06:00-09:00 MON-FRI, 12:00-19:00 MON-FRI, 12:00-17:00 SAT		
28 A		NO WORK 06:00-09:00 MON-FRI, 12:00-19:00 MON-FRI, 12:00-17:00 SAT		
30	C-6	NO WORK 06:00-09:00 MON-FRI, 12:00-19:00 MON-FRI, 12:00-17:00 SAT		
31	C-5	NO WORK 06:00-09:00 MON-FRI, 12:00-19:00 MON-FRI, 12:00-17:00 SAT		
33	A-1	23:00 TO 05:00 ONLY, EXCEPT WORK MAY CONTINUE TO 07:00 SAT OR 09:00 SUN		
33	CP1R	23:00 TO 05:00 ONLY, EXCEPT WORK MAY CONTINUE TO 07:00 SAT OR 09:00 SUN		
33	CP2L	23:00 TO 05:00 ONLY		
33	D-4	23:00 TO 05:00 ONLY, EXCEPT WORK MAY CONTINUE TO 07:00 SAT OR 09:00 SUN		
33	TB	00:00 TO 04:00 ONLY		
34	CP1R	NO WORK 07:00-09:00 MON-FRI, 14:00-19:00 MON-FRI		
34 A		NO WORK 07:00-09:00 MON-FRI, 14:00-19:00 MON-FRI		
34 A		23:00 TO 05:00 ONLY		
35	D-3 & D-4	19:00 TO 06:00 ONLY		
36	A-3	23:00 TO 05:00 ONLY		
36	CP1R	NO WORK 07:00-09:00 MON-FRI, 14:00-19:00 MON-FRI		
36	ТВ	00:00 TO 04:00 ONLY		
36		19:00 TO 06:00 ONLY		
36 A	_	23:00 TO 05:00 ONLY		
36 A		NO WORK 07:00-09:00 MON-FRI, 14:00-19:00 MON-FRI		
36 A		00:00 TO 04:00 ONLY		
37	A-3	23:00 TO 05:00 ONLY		
37	CP1R	23:00 TO 05:00 ONLY		
01	1 01 111	20.00 TO 00.00 OIVET		

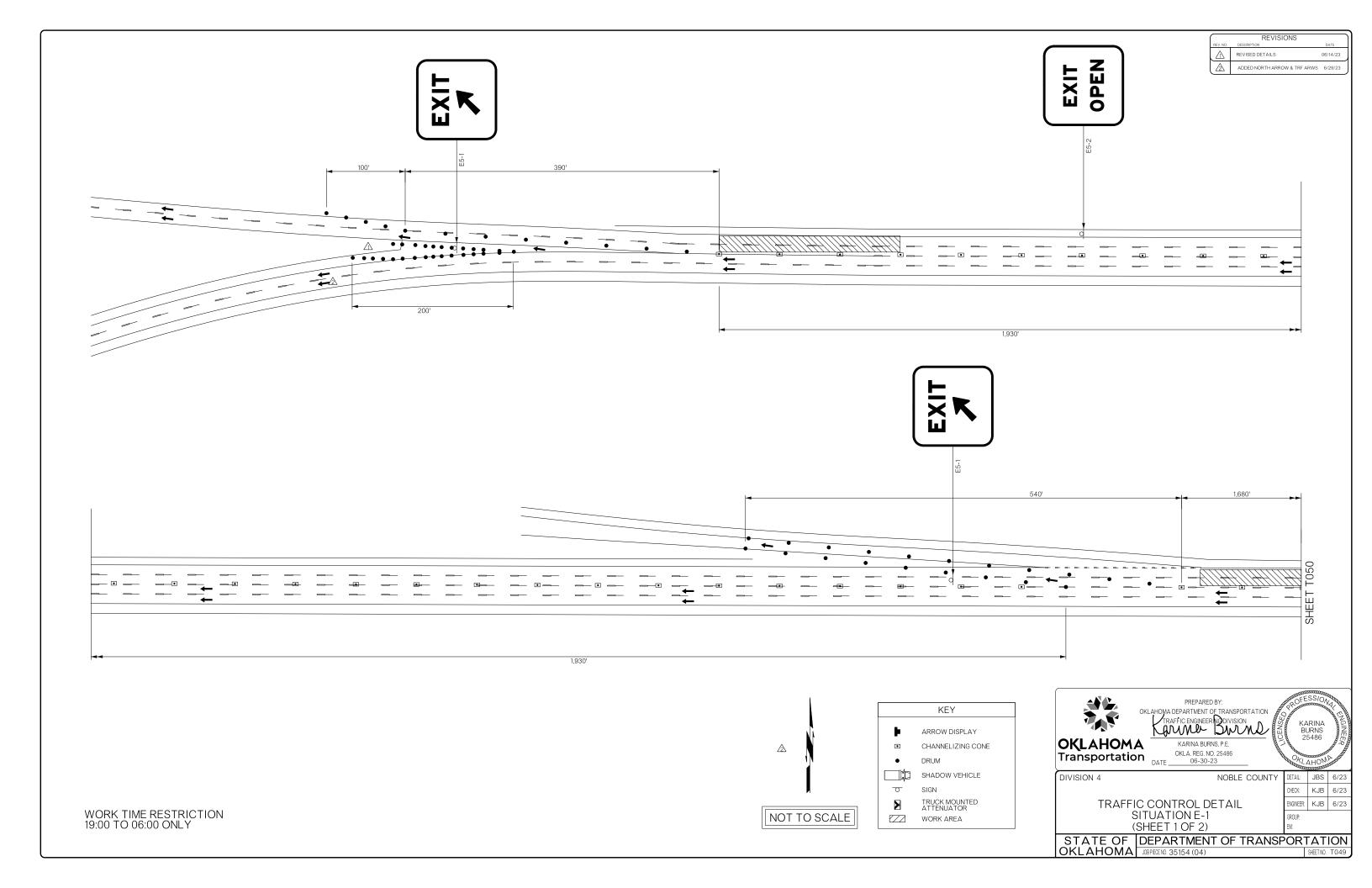
	SIGN	SETUP	RESTRICTION
-	37	TB	00:00 TO 04:00 ONLY
	40	A-2	23:00 TO 05:00 ONLY, EXCEPT WORK MAY CONTINUE TO 07:00 SAT OR 09:00 SUN
	40	A-3	23:00 TO 05:00 ONLY
	40	TB	00:00 TO 04:00 ONLY
	40	D-3 & D-4	19:00 TO 06:00 ONLY
	40 A	A-2	23:00 TO 05:00 ONLY, EXCEPT WORK MAY CONTINUE TO 07:00 SAT OR 09:00 SUN
	40 A	A-3	23:00 TO 05:00 ONLY
	40 A	TB	00:00 TO 04:00 ONLY
	41	C-2	NO WORK 06:00-09:00 MON-FRI, 12:00-19:00 MON-FRI, 12:00-17:00 SAT
	42	C-6	NO WORK 06:00-09:00 MON-FRI, 12:00-19:00 MON-FRI, 12:00-17:00 SAT
	43	C-1	NO WORK 06:00-09:00 MON-FRI, 12:00-19:00 MON-FRI, 12:00-17:00 SAT
	45 A	A-2	23:00 TO 05:00 ONLY, EXCEPT WORK MAY CONTINUE TO 07:00 SAT OR 09:00 SUN
	45 A	Δ-3	23:00 TO 05:00 ONLY
	45 A	D-3	19:00 TO 06:00 ONLY
	45 A	TB	00:00 TO 04:00 ONLY
	46	Δ-2	23:00 TO 05:00 ONLY, EXCEPT WORK MAY CONTINUE TO 07:00 SAT OR 09:00 SUN
	46	A-3	23:00 TO 05:00 ONLY
	46	TB	00:00 TO 04:00 ONLY
	48	Α-3	23:00 TO 05:00 ONLY
7	48	CP2R	19:00 TO 07:00 ONLY
	48	D-3	23:00 TO 05:00 ONLY
	48	TB	00:00 TO 04:00 ONLY
	49	C-4	NO WORK 06:00-09:00 MON-FRI, 12:00-19:00 MON-FRI, 12:00-17:00 SAT
	50	CP2R	23:00 TO 05:00 ONLY
	52	D-4	19:00 TO 06:00 ONLY
	55	C-2	NO WORK 06:00-09:00 MON-FRI, 12:00-19:00 MON-FRI, 12:00-17:00 SAT
	56		23:00 TO 05:00 ONLY, EXCEPT WORK MAY CONTINUE TO 07:00 SAT OR 09:00 SUN
	57		19:00 TO 06:00 ONLY
	58		19:00 TO 06:00 ONLY
	61	D-3 & D-4	19:00 TO 06:00 ONLY
	62	D-4	23:00 TO 05:00 ONLY, EXCEPT WORK MAY CONTINUE TO 07:00 SAT OR 09:00 SUN
	63	D-4	23:00 TO 05:00 ONLY, EXCEPT WORK MAY CONTINUE TO 07:00 SAT OR 08:00 SUN
	65	D-4	23:00 TO 05:00 ONLY, EXCEPT WORK MAY CONTINUE TO 07:00 SAT OR 08:00 SUN
7	67		19:00 TO 06:00 ONLY
7	67	TB	00:00 TO 04:00 ONLY
	67	D-3 & D-4	23:00 TO 05:00 ONLY, EXCEPT WORK MAY CONTINUE TO 07:00 SAT OR 08:00 SUN
	70	D-4	23:00 TO 05:00 ONLY, EXCEPT WORK MAY CONTINUE TO 07:00 SAT OR 08:00 SUN
	71	D-4	23:00 TO 05:00 ONLY, EXCEPT WORK MAY CONTINUE TO 07:00 SAT OR 08:00 SUN
	74	D-4	23:00 TO 05:00 ONLY, EXCEPT WORK MAY CONTINUE TO 07:00 SAT OR 08:00 SUN
	75	D-4	23:00 TO 05:00 ONLY, EXCEPT WORK MAY CONTINUE TO 07:00 SAT OR 08:00 SUN

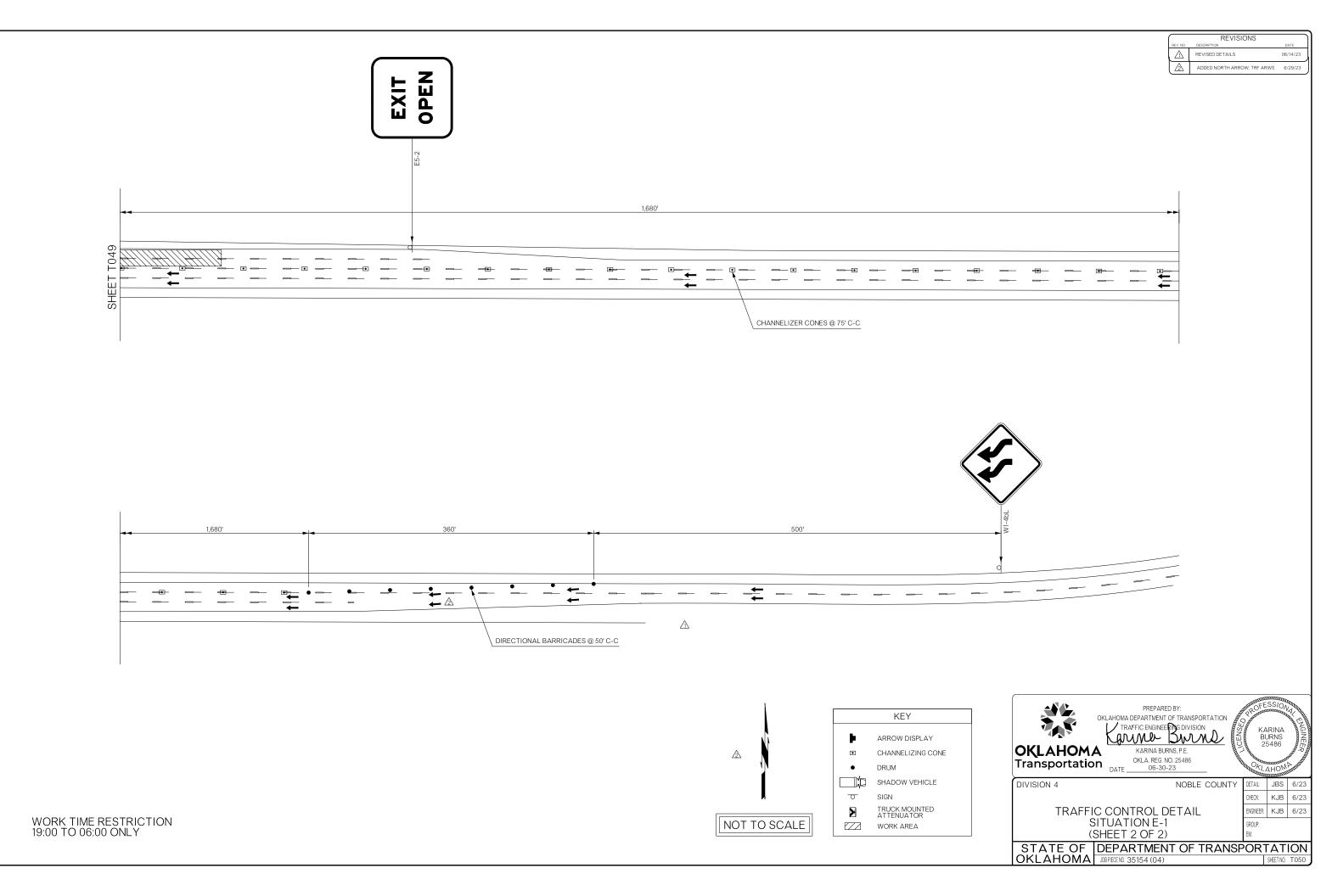
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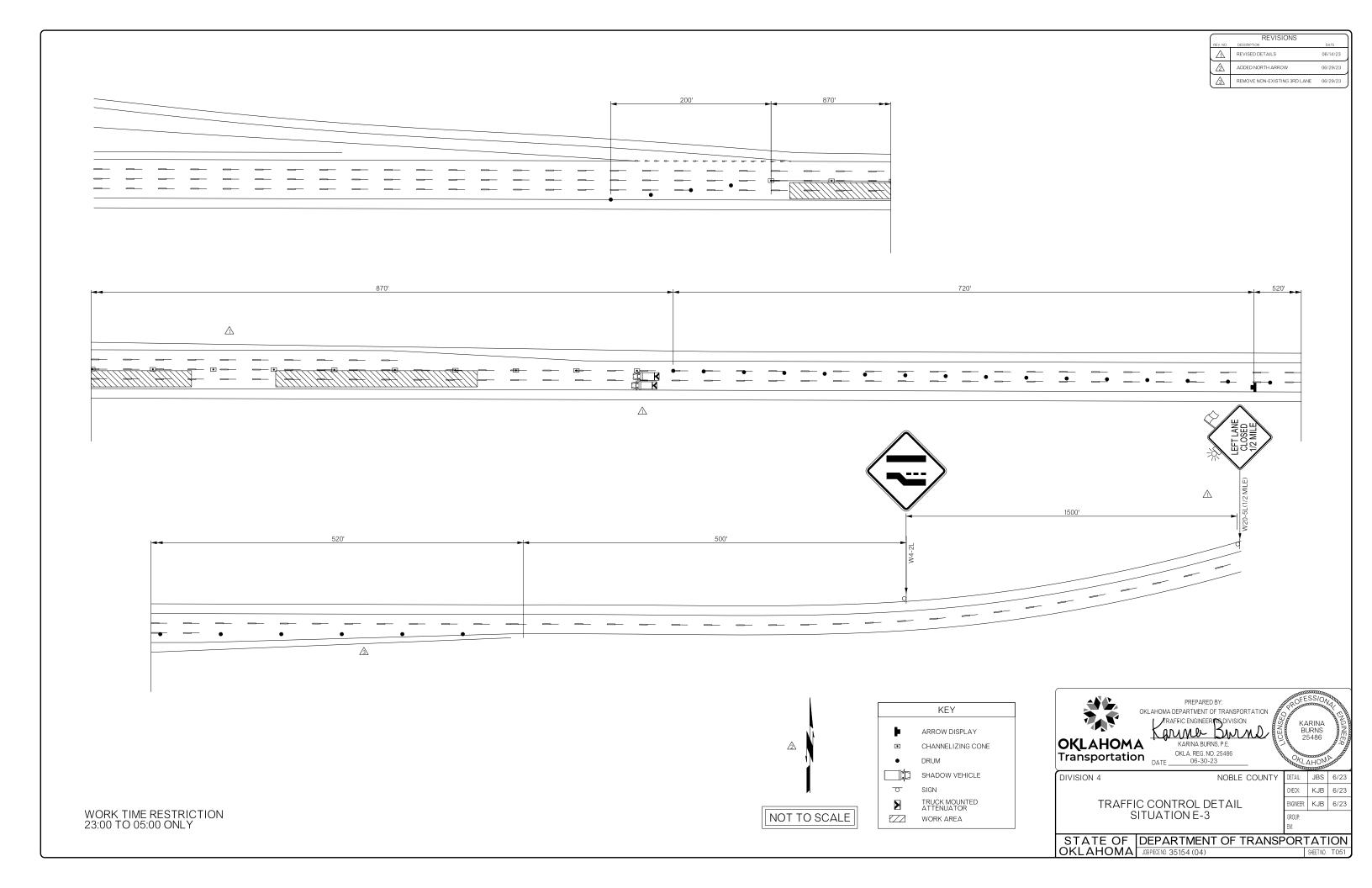


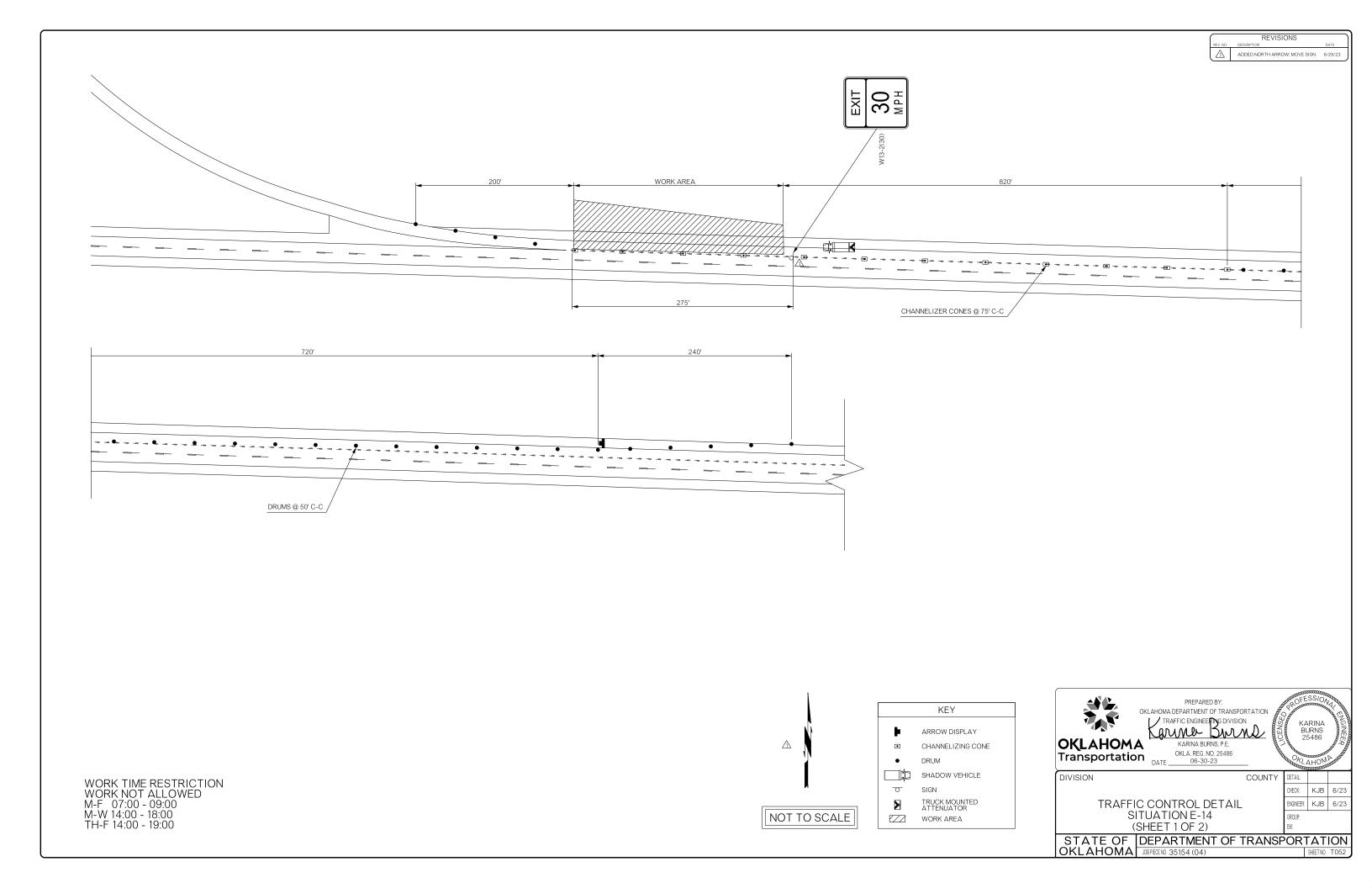
NOBLE COUNTY

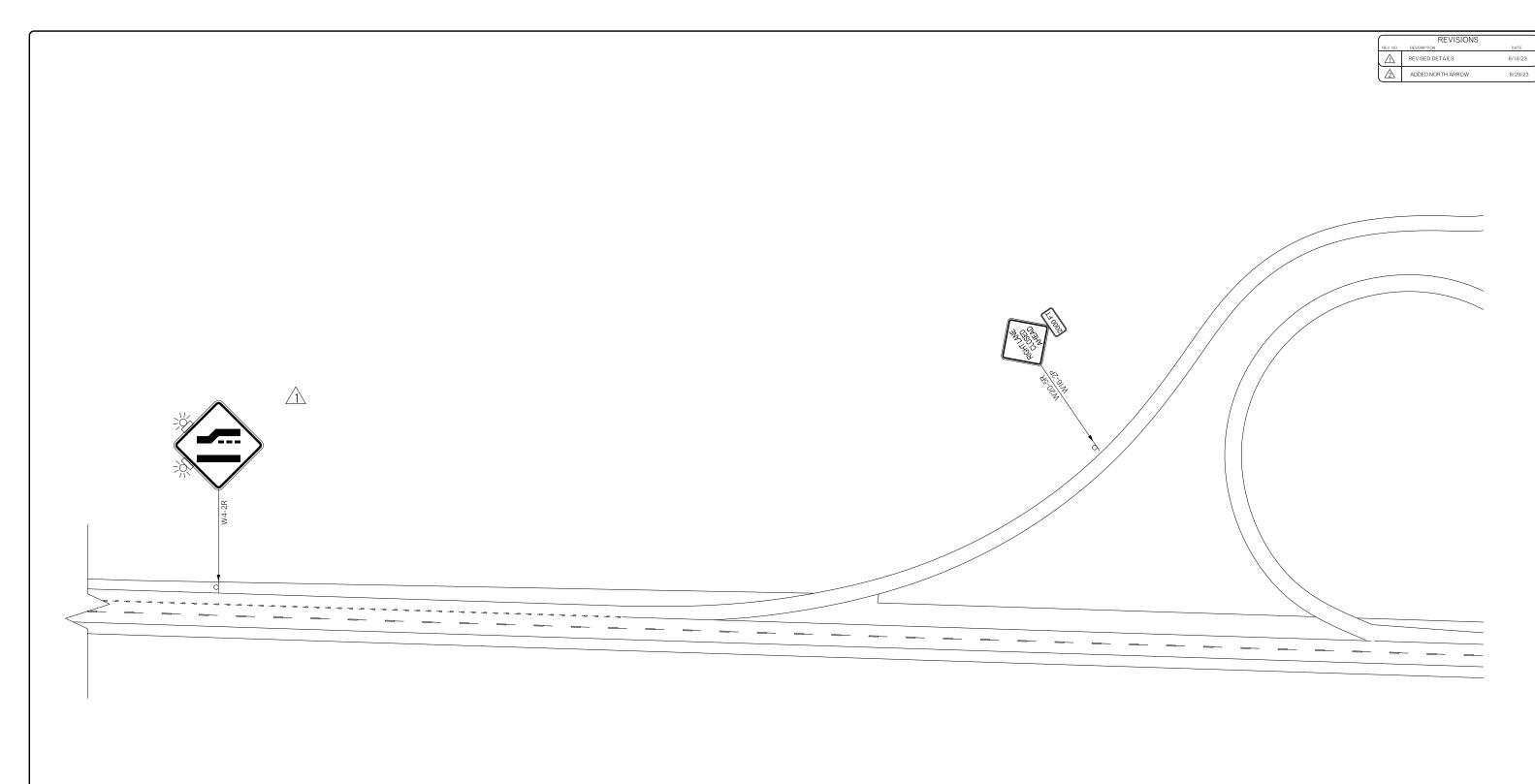
ENGINEER: KJB 5/23
GROUP: WORK TIME RESTRICTIONS

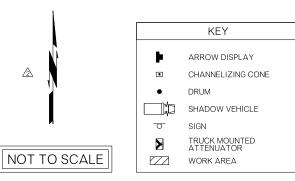


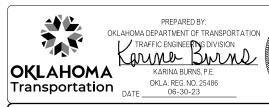




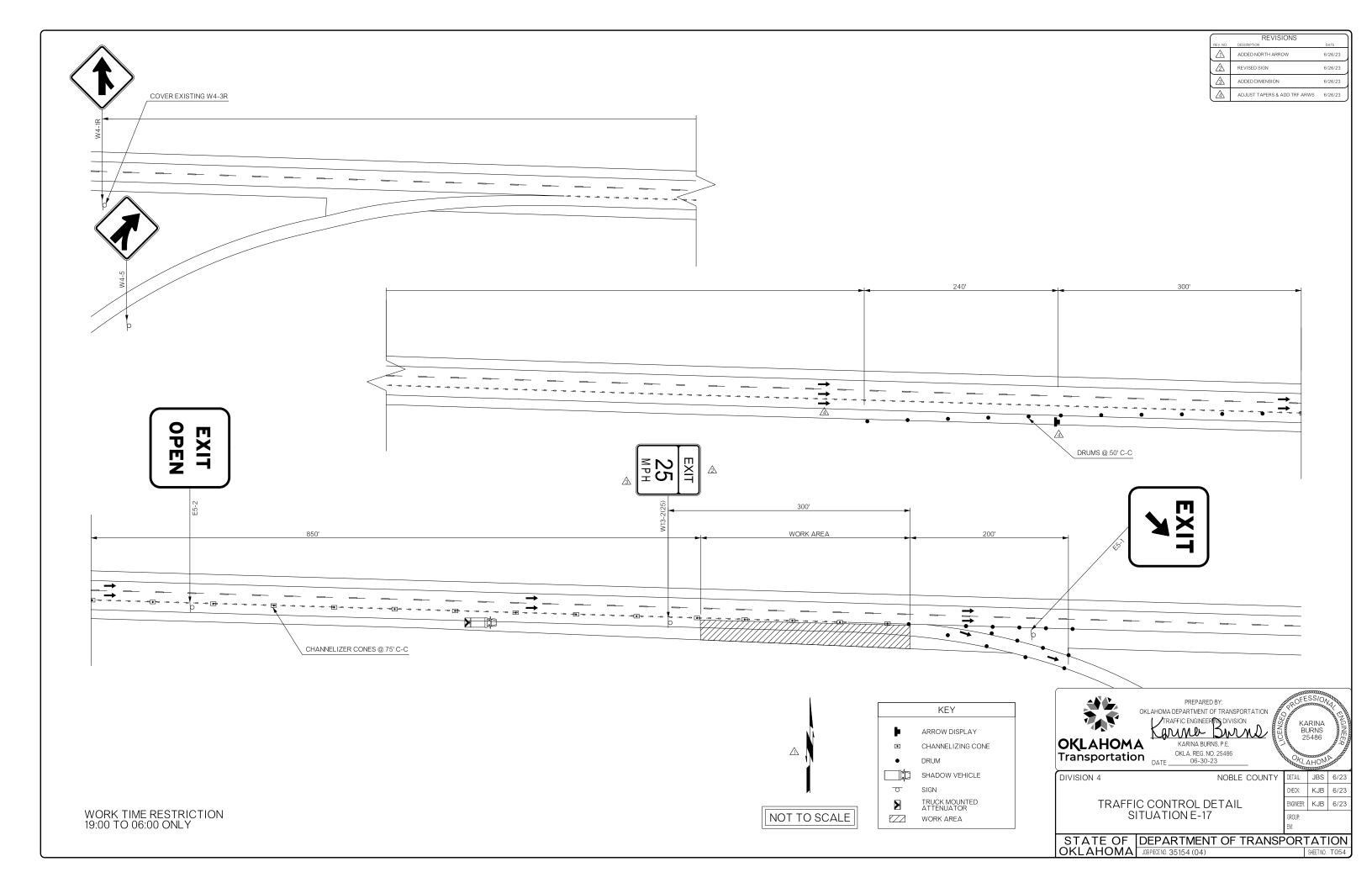


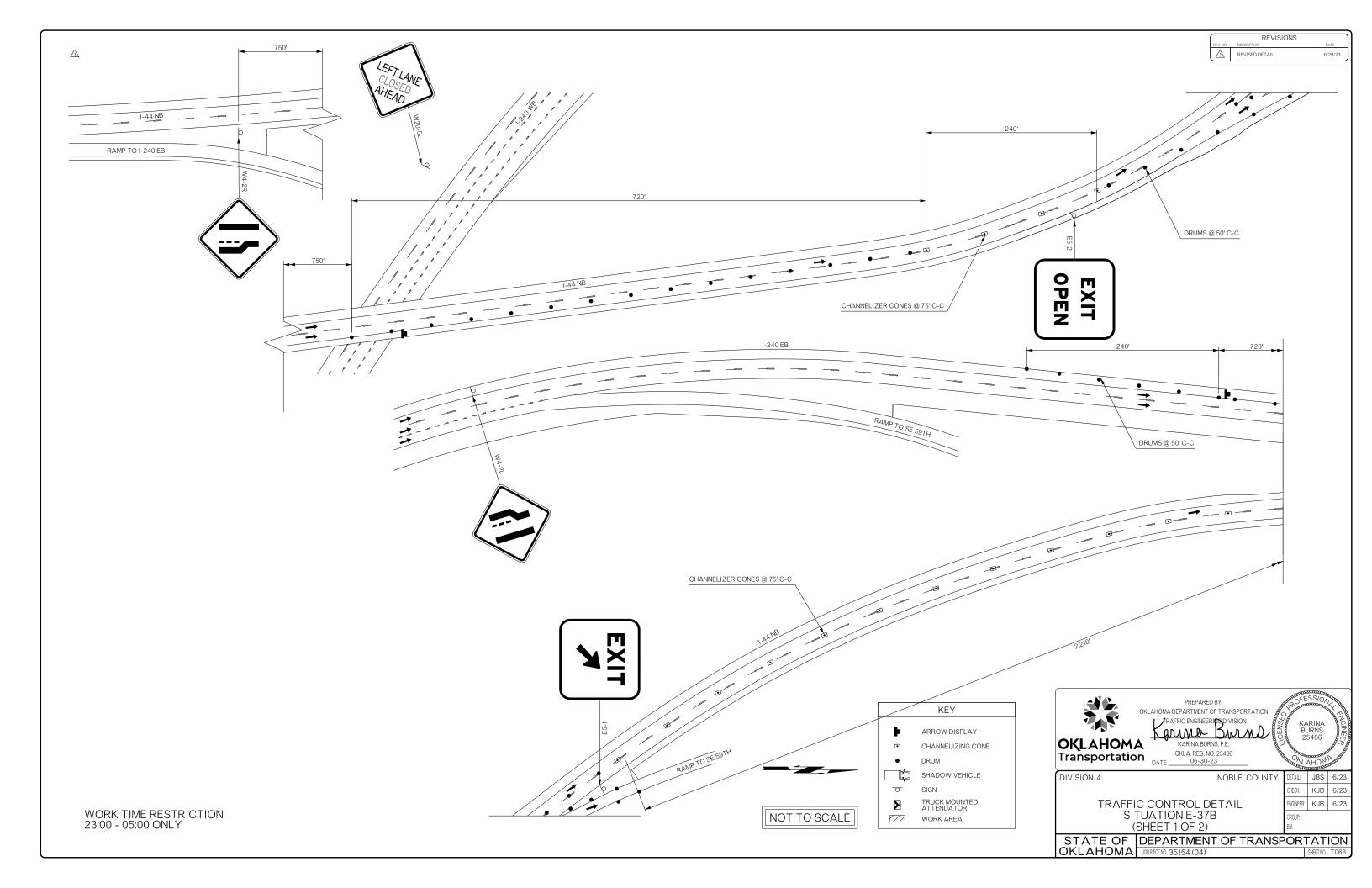


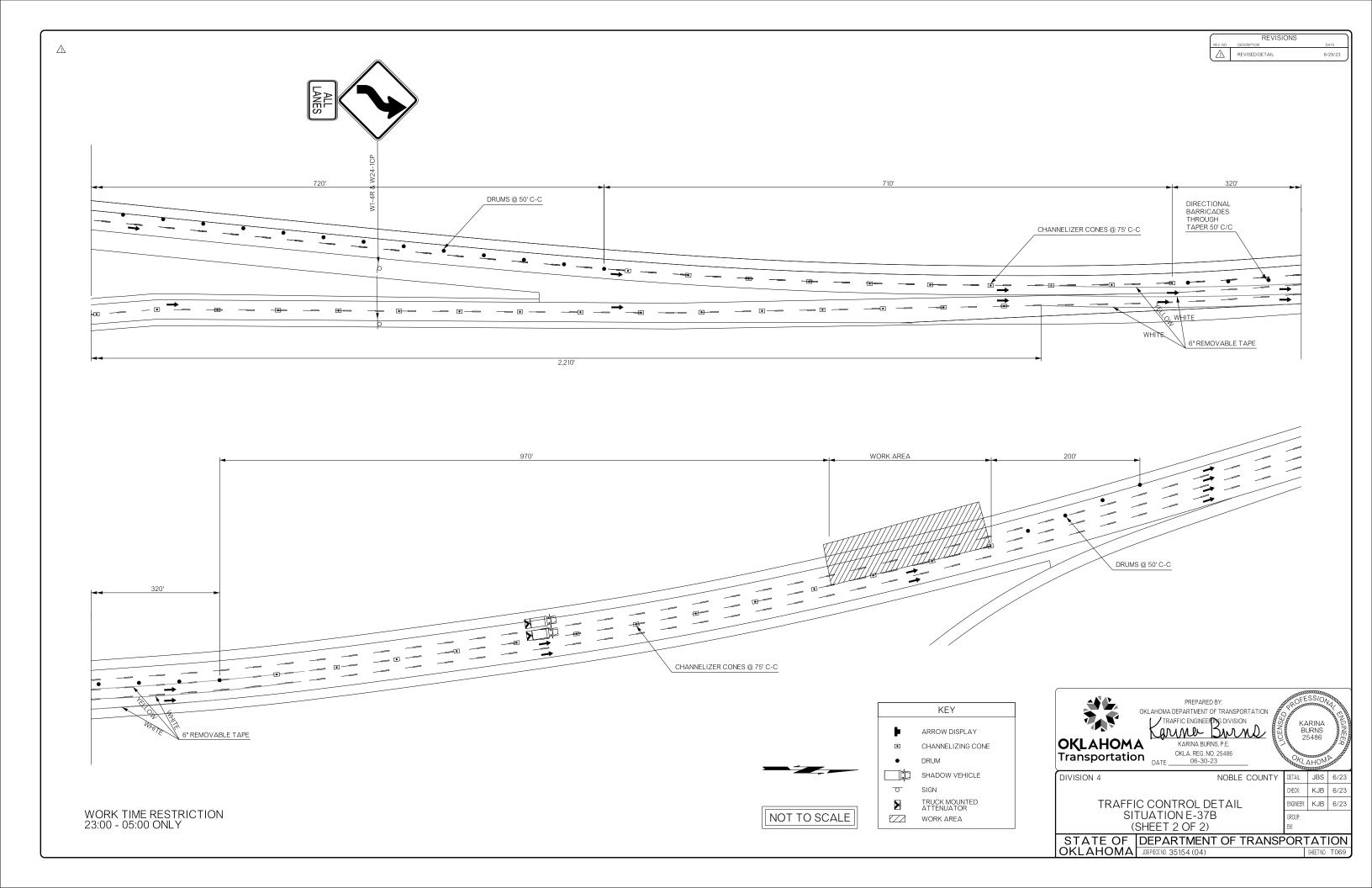


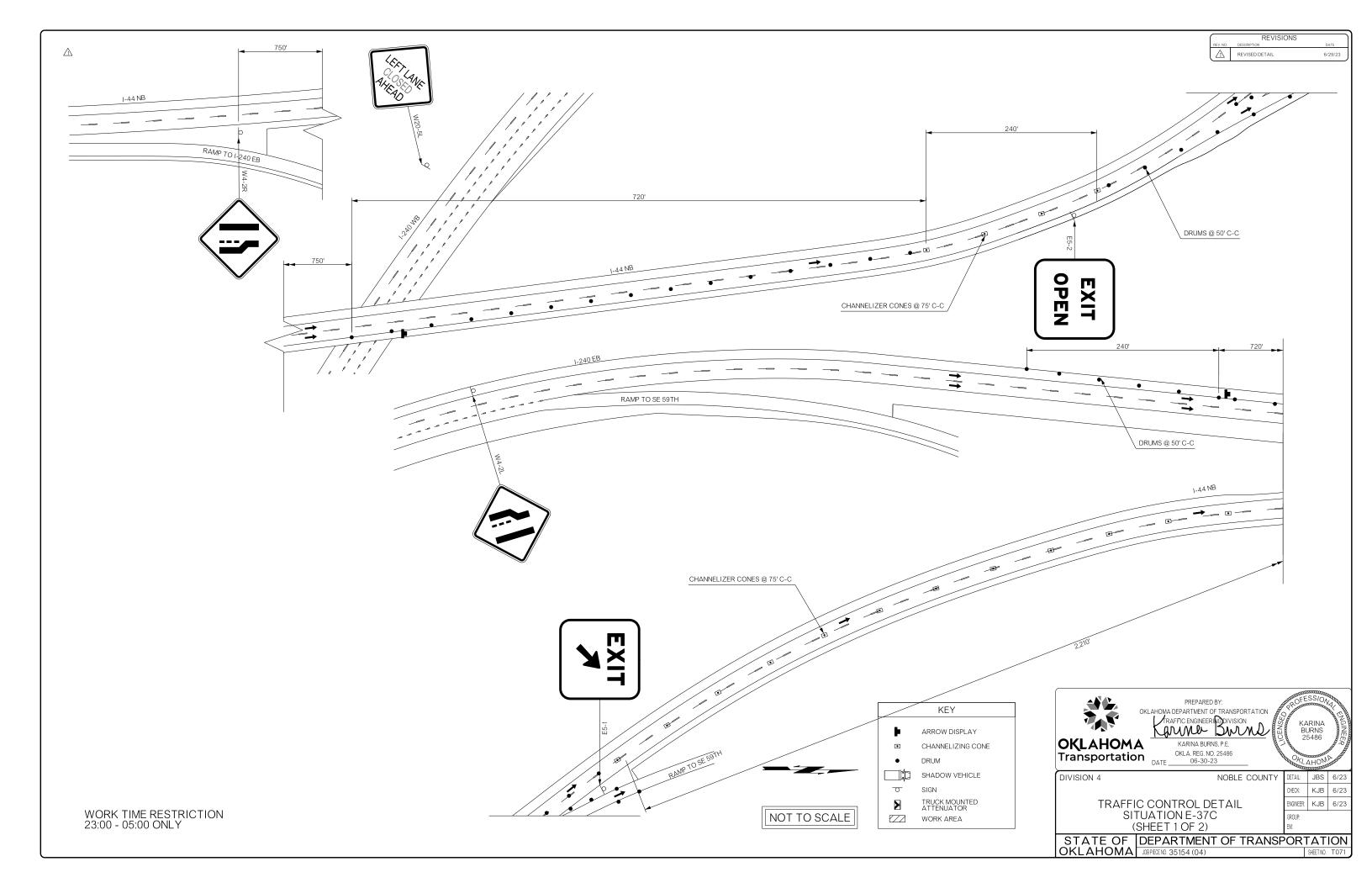


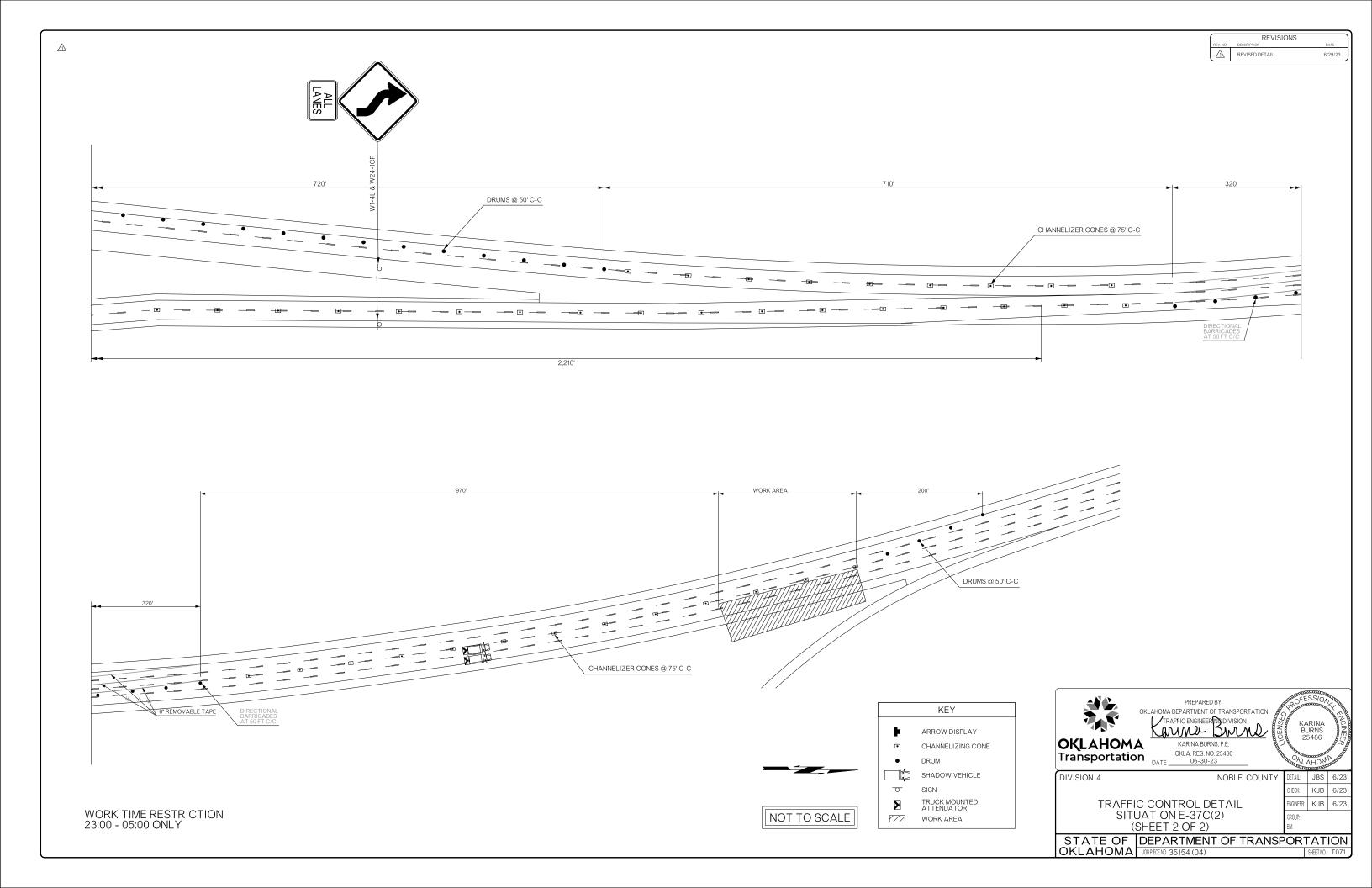
DIVISION 4 NOBLE COUNTY TRAFFIC CONTROL DETAIL SITUATION E-14 (SHEET 2 OF 2) ENGINEER: KJB 6/23

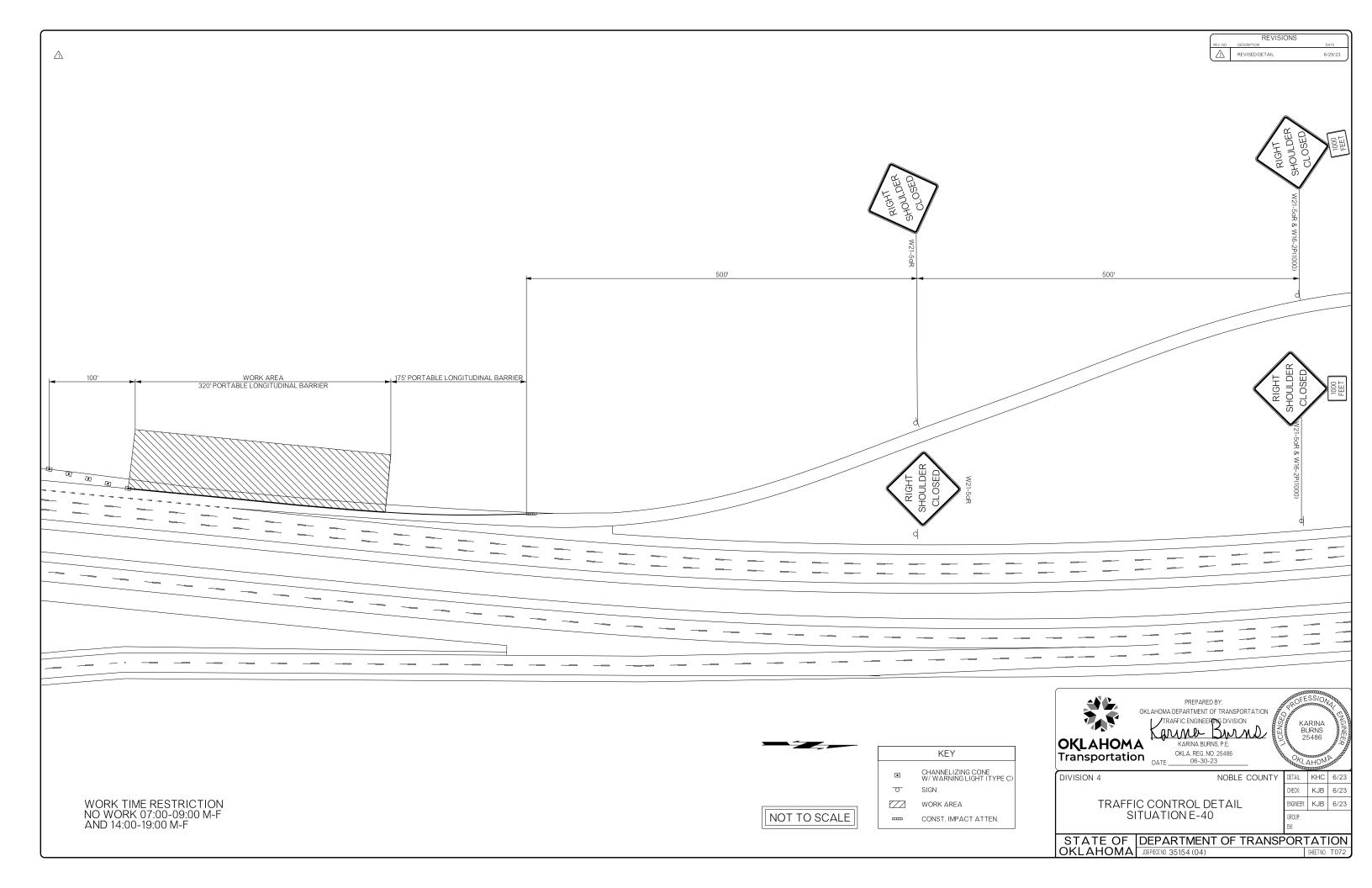


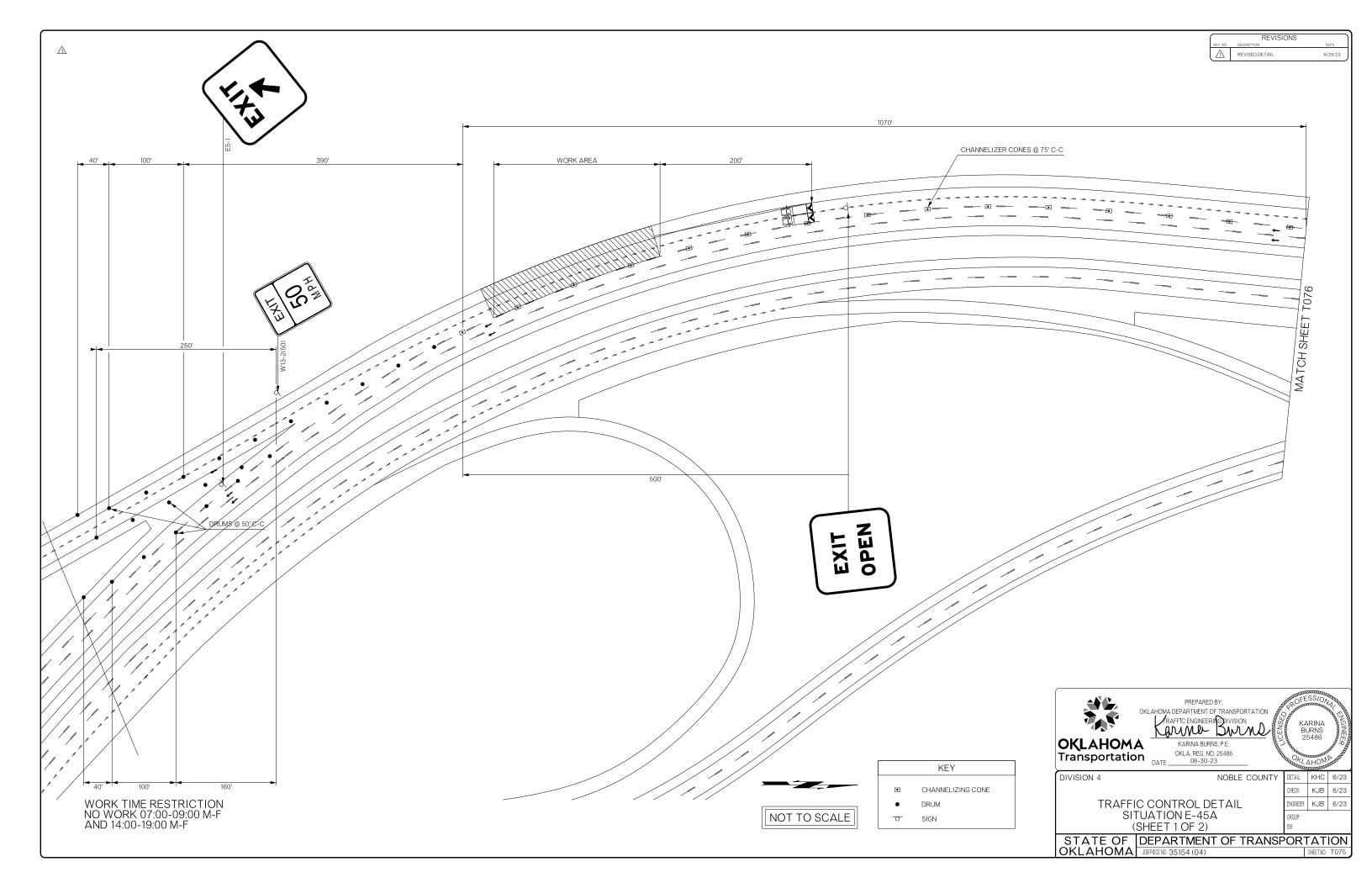


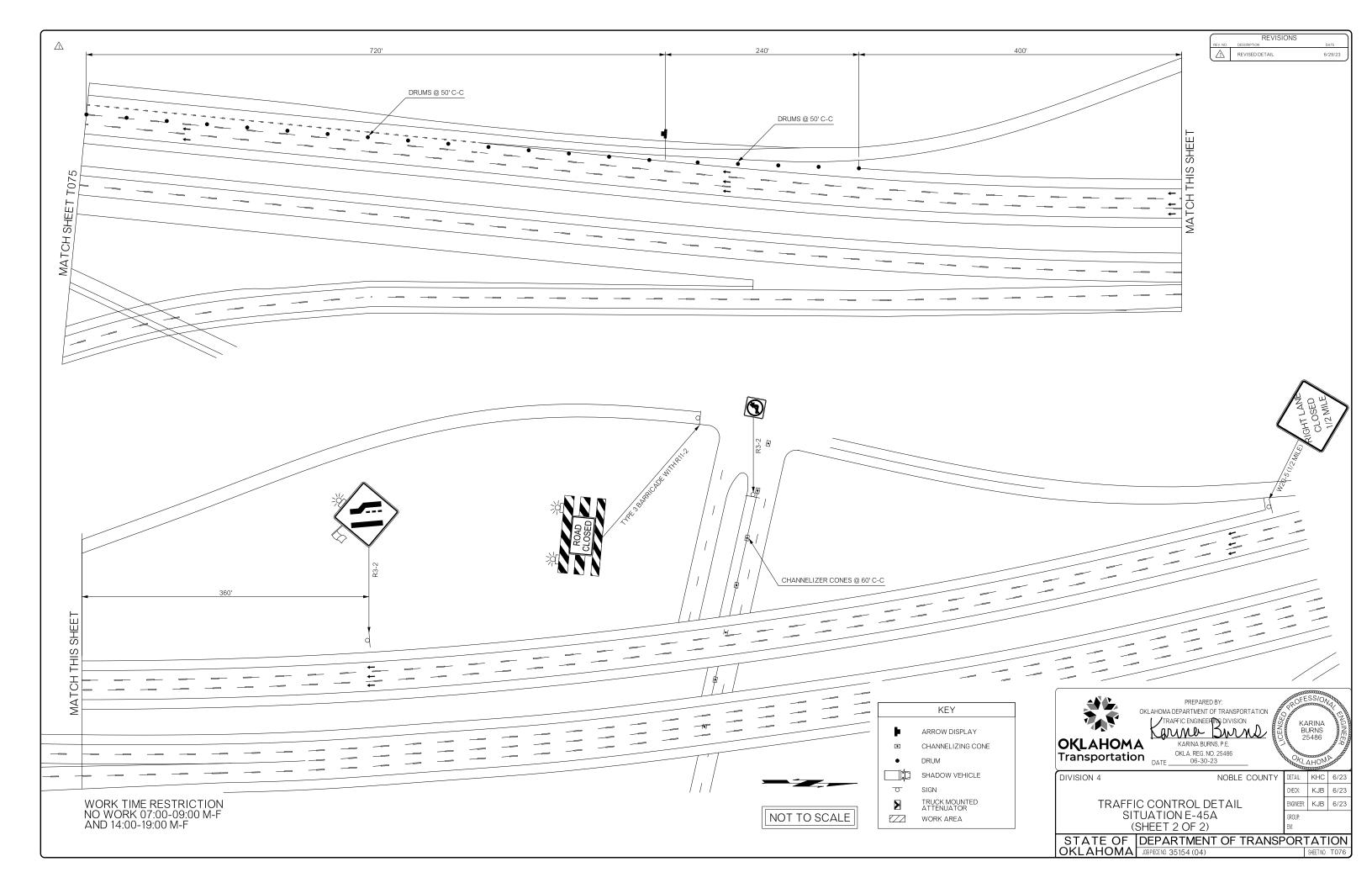






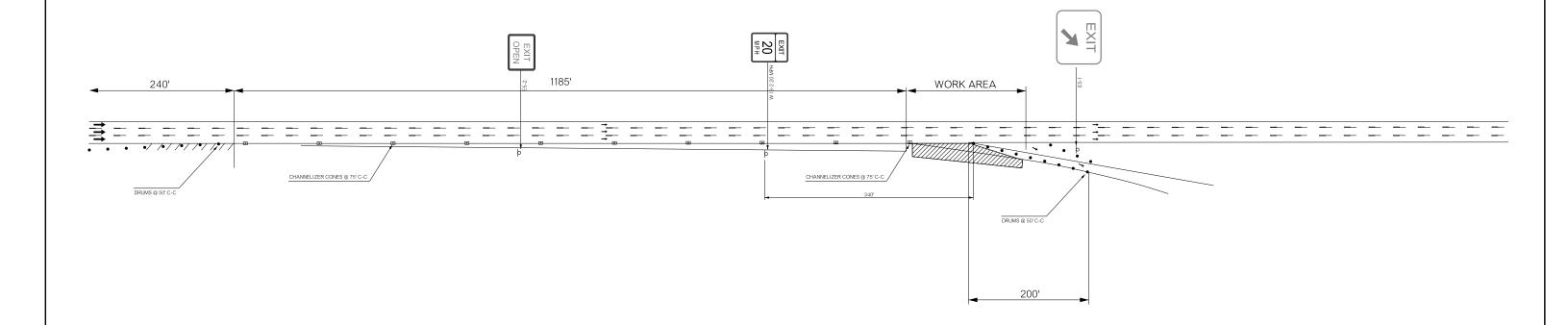


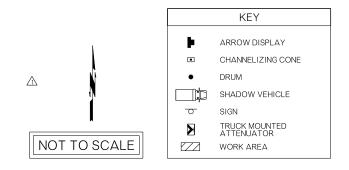


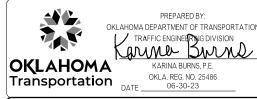


REVISIONS ADD NORTH ARW, TRF ARWS, DIMS 6/29/23

SHORT TERM STATIONARY







OKLAHOMA DEPARTMENT OF TRANSPORTATION

KARINA BURNS 25486

CHECK: KJB 6/23

DIVISION 4 NOBLE COUNTY

TRAFFIC CONTROL DETAIL SITUATION E-67

TRAFFI	ENGINEER:	KJB	6/23			
SI	ITUATION E-67		GROUP: EM:			
	DEPARTMENT OF TRANSPORTATION					
OKLAHOMA	JOB PIECE NO. 35154 (04)		SHEET NO.	T085		

WORK TIME RESTRICTION 19:00 TO 06:00 ONLY