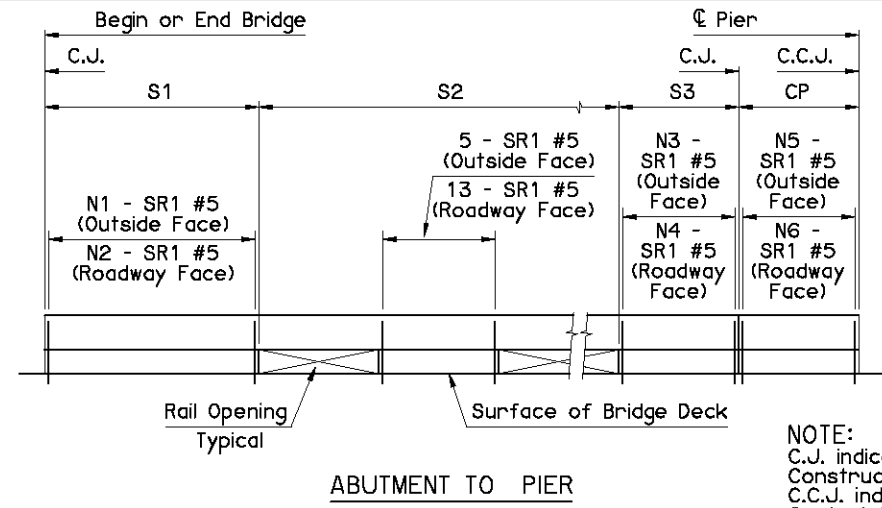
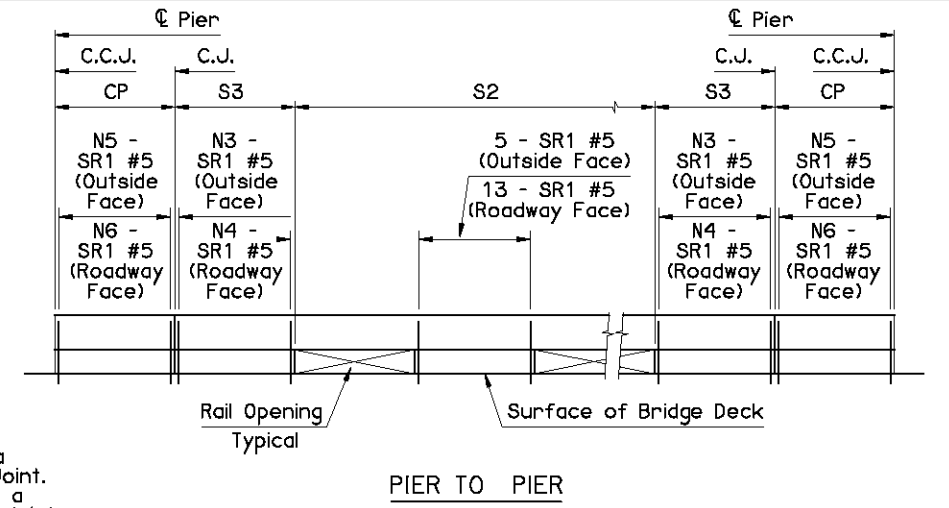


ABUTMENT TO ABUTMENT



ABUTMENT TO PIER



PIER TO PIER

NOTE:
C.J. indicates a Construction Joint.
C.C.J. indicates a Control Crack Joint.
For additional detail of Concrete Traffic Rail, see Std. TR4-2.

CONCRETE TRAFFIC RAIL ELEVATION

ROLLED BEAMS CONCRETE TRAFFIC RAIL WITH OPENINGS SCHEDULE																											
SPAN	SPAN TYPE																										
	ABUTMENT TO ABUTMENT				ABUTMENT TO PIER								PIER TO PIER														
	S1	S2			N1	N2	S1	N1	N2	S2			S3	N3	N4	CP	N5	N6	S2			S3	N3	N4	CP	N5	N6
30'	7'-9"	3 Spa. @ 5'-0" = 15'-0"			8	20	6'-3"	7	16	3 Spa. @ 5'-0" = 15'-0"			5'-0"	5	13	4'-0"	5	10	1 Spa. @ 5'-0" = 5'-0"			8'-6"	8	22	4'-0"	5	10
35'	5'-3"	5 Spa. @ 5'-0" = 25'-0"			6	14	10'-0"	10	25	3 Spa. @ 5'-0" = 15'-0"			6'-3"	7	16	4'-0"	5	10	3 Spa. @ 5'-0" = 15'-0"			6'-0"	6	16	4'-0"	5	10
40'	7'-9"	5 Spa. @ 5'-0" = 25'-0"			8	20	6'-3"	7	16	5 Spa. @ 5'-0" = 25'-0"			5'-0"	5	13	4'-0"	5	10	3 Spa. @ 5'-0" = 15'-0"			8'-6"	8	22	4'-0"	5	10
45'	5'-3"	7 Spa. @ 5'-0" = 35'-0"			6	14	10'-0"	10	25	5 Spa. @ 5'-0" = 25'-0"			6'-3"	7	16	4'-0"	5	10	5 Spa. @ 5'-0" = 25'-0"			6'-0"	6	16	4'-0"	5	10
50'	7'-9"	7 Spa. @ 5'-0" = 35'-0"			8	20	6'-3"	7	16	7 Spa. @ 5'-0" = 35'-0"			5'-0"	5	13	4'-0"	5	10	5 Spa. @ 5'-0" = 25'-0"			8'-6"	8	22	4'-0"	5	10
55'	5'-3"	9 Spa. @ 5'-0" = 45'-0"			6	14	10'-0"	10	25	7 Spa. @ 5'-0" = 35'-0"			6'-3"	7	16	4'-0"	5	10	7 Spa. @ 5'-0" = 35'-0"			6'-0"	6	16	4'-0"	5	10
60'	7'-9"	9 Spa. @ 5'-0" = 45'-0"			8	20	6'-3"	7	16	9 Spa. @ 5'-0" = 45'-0"			5'-0"	5	13	4'-0"	5	10	7 Spa. @ 5'-0" = 35'-0"			8'-6"	8	22	4'-0"	5	10
65'	5'-3"	11 Spa. @ 5'-0" = 55'-0"			6	14	9'-3"	9	24	9 Spa. @ 5'-0" = 45'-0"			5'-0"	5	13	6'-0"	6	15	7 Spa. @ 5'-0" = 35'-0"			9'-0"	9	23	6'-0"	6	15
70'	7'-9"	11 Spa. @ 5'-0" = 55'-0"			8	20	12'-0"	11	30	9 Spa. @ 5'-0" = 45'-0"			7'-3"	7	19	6'-0"	6	15	9 Spa. @ 5'-0" = 45'-0"			6'-6"	7	17	6'-0"	6	15
75'	5'-3"	13 Spa. @ 5'-0" = 65'-0"			6	14	9'-3"	9	24	11 Spa. @ 5'-0" = 55'-0"			5'-0"	5	13	6'-0"	6	15	9 Spa. @ 5'-0" = 45'-0"			9'-0"	9	23	6'-0"	6	15
80'	7'-9"	13 Spa. @ 5'-0" = 65'-0"			8	20	12'-0"	11	30	11 Spa. @ 5'-0" = 55'-0"			7'-3"	7	19	6'-0"	6	15	11 Spa. @ 5'-0" = 55'-0"			6'-6"	7	17	6'-0"	6	15
85'	5'-3"	15 Spa. @ 5'-0" = 75'-0"			6	14	9'-3"	9	24	13 Spa. @ 5'-0" = 65'-0"			5'-0"	5	13	6'-0"	6	15	11 Spa. @ 5'-0" = 55'-0"			9'-0"	9	23	6'-0"	6	15
90'	7'-9"	15 Spa. @ 5'-0" = 75'-0"			8	20	12'-0"	11	30	13 Spa. @ 5'-0" = 65'-0"			7'-3"	7	19	6'-0"	6	15	13 Spa. @ 5'-0" = 65'-0"			6'-6"	7	17	6'-0"	6	15
95'	5'-3"	17 Spa. @ 5'-0" = 85'-0"			6	14	9'-3"	9	24	15 Spa. @ 5'-0" = 75'-0"			5'-0"	5	13	6'-0"	6	15	13 Spa. @ 5'-0" = 65'-0"			9'-0"	9	23	6'-0"	6	15
100'	7'-9"	17 Spa. @ 5'-0" = 85'-0"			8	20	12'-0"	11	30	15 Spa. @ 5'-0" = 75'-0"			7'-3"	7	19	6'-0"	6	15	15 Spa. @ 5'-0" = 75'-0"			6'-6"	7	17	6'-0"	6	15

ROLLED BEAMS CONCRETE TRAFFIC RAIL WITH OPENINGS SR1 BAR LIST							
SPAN	EPOXY COATED REINFORCING				SPAN TYPE		
					ABUTMENT TO ABUTMENT	ABUTMENT TO PIER	PIER TO PIER
	MARK	SIZE	FORM	LENGTH	NO.	NO.	NO.
30'	SR1	#5	BNT.	4'-1"	148	148	180
35'	SR1	#5	BNT.	4'-1"	152	182	184
40'	SR1	#5	BNT.	4'-1"	184	184	216
45'	SR1	#5	BNT.	4'-1"	188	218	220
50'	SR1	#5	BNT.	4'-1"	220	220	252
55'	SR1	#5	BNT.	4'-1"	224	254	256
60'	SR1	#5	BNT.	4'-1"	256	256	288
65'	SR1	#5	BNT.	4'-1"	260	288	320
70'	SR1	#5	BNT.	4'-1"	292	320	324
75'	SR1	#5	BNT.	4'-1"	296	324	356
80'	SR1	#5	BNT.	4'-1"	328	356	360
85'	SR1	#5	BNT.	4'-1"	332	360	392
90'	SR1	#5	BNT.	4'-1"	364	392	396
95'	SR1	#5	BNT.	4'-1"	368	396	428
100'	SR1	#5	BNT.	4'-1"	400	428	432

NOTE:
For bar bend, see Std. TR4-2.

APPROVED BY BRIDGE ENGINEER *Scott J. Smith* DATE *4/2/10*

OKLAHOMA DEPT. OF TRANSPORTATION
BRIDGE STANDARD (ENGLISH)
CONCRETE TRAFFIC RAIL WITH OPENINGS
ROLLED BEAMS
INTEGRAL

2009 SPECIFICATIONS | B40-I-TR4-0-RB | 01E
B-221E