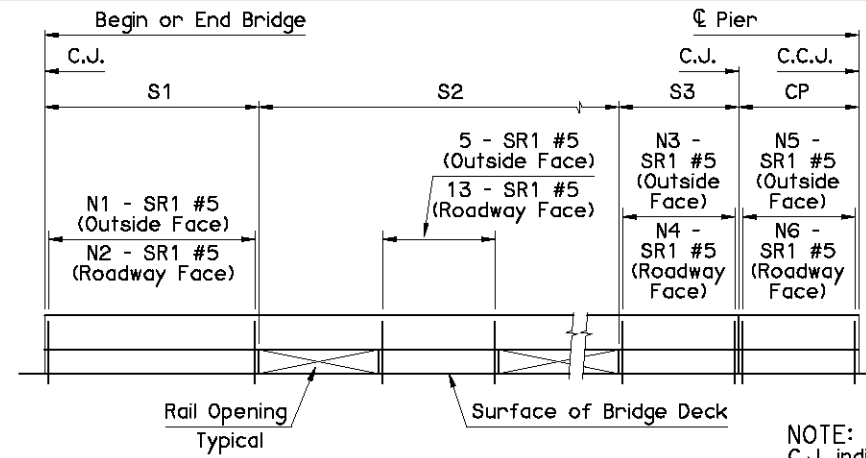
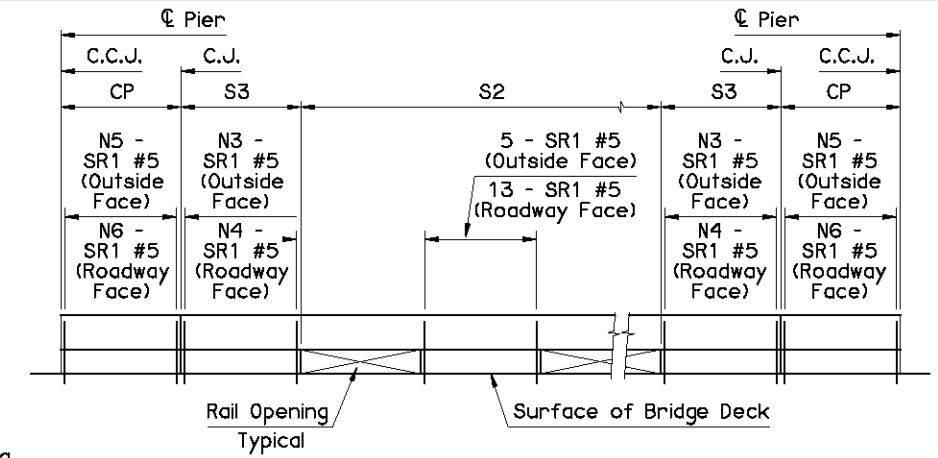


ABUTMENT TO ABUTMENT



ABUTMENT TO PIER



PIER TO PIER

**CONCRETE TRAFFIC RAIL ELEVATION**

NOTE:  
C.J. indicates a Construction Joint.  
C.C.J. indicates a Control Crack Joint.  
For additional detail of Concrete Traffic Rail, see Std. TR4-2.

TYPE II AND TYPE B P.C. BEAMS CONCRETE TRAFFIC RAIL WITH OPENINGS SCHEDULE																											
SPAN	SPAN TYPE																										
	ABUTMENT TO ABUTMENT				ABUTMENT TO PIER								PIER TO PIER														
	S1	S2			N1	N2	S1	N1	N2	S2			S3	N3	N4	CP	N5	N6	S2			S3	N3	N4	CP	N5	N6
30'	7'-9"	3 Spa. @ 5'-0" = 15'-0"			8	20	6'-3"	7	16	3 Spa. @ 5'-0" = 15'-0"			5'-0"	5	13	4'-0"	5	10	1 Spa. @ 5'-0" = 5'-0"			8'-6"	8	22	4'-0"	5	10
35'	5'-3"	5 Spa. @ 5'-0" = 25'-0"			6	14	10'-0"	10	25	3 Spa. @ 5'-0" = 15'-0"			6'-3"	7	16	4'-0"	5	10	3 Spa. @ 5'-0" = 15'-0"			6'-0"	6	16	4'-0"	5	10
40'	7'-9"	5 Spa. @ 5'-0" = 25'-0"			8	20	6'-3"	7	16	5 Spa. @ 5'-0" = 25'-0"			5'-0"	5	13	4'-0"	5	10	3 Spa. @ 5'-0" = 15'-0"			8'-6"	8	22	4'-0"	5	10
45'	5'-3"	7 Spa. @ 5'-0" = 35'-0"			6	14	10'-0"	10	25	5 Spa. @ 5'-0" = 25'-0"			6'-3"	7	16	4'-0"	5	10	5 Spa. @ 5'-0" = 25'-0"			6'-0"	6	16	4'-0"	5	10
50'	7'-9"	7 Spa. @ 5'-0" = 35'-0"			8	20	6'-3"	7	16	7 Spa. @ 5'-0" = 35'-0"			5'-0"	5	13	4'-0"	5	10	5 Spa. @ 5'-0" = 25'-0"			8'-6"	8	22	4'-0"	5	10
55'	5'-3"	9 Spa. @ 5'-0" = 45'-0"			6	14	10'-0"	10	25	7 Spa. @ 5'-0" = 35'-0"			6'-3"	7	16	4'-0"	5	10	7 Spa. @ 5'-0" = 35'-0"			6'-0"	6	16	4'-0"	5	10

TYPE II AND TYPE B P.C. BEAMS CONCRETE TRAFFIC RAIL WITH OPENINGS SR1 BAR LIST							
SPAN	EPOXY COATED REINFORCING				SPAN TYPE		
					ABUTMENT TO ABUTMENT	ABUTMENT TO PIER	PIER TO PIER
	MARK	SIZE	FORM	LENGTH	NO.	NO.	NO.
30'	SR1	#5	BNT.	4'-1"	148	148	180
35'	SR1	#5	BNT.	4'-1"	152	182	184
40'	SR1	#5	BNT.	4'-1"	184	184	216
45'	SR1	#5	BNT.	4'-1"	188	218	220
50'	SR1	#5	BNT.	4'-1"	220	220	252
55'	SR1	#5	BNT.	4'-1"	224	254	256

TYPE III AND TYPE C P.C. BEAMS CONCRETE TRAFFIC RAIL WITH OPENINGS SCHEDULE																											
SPAN	SPAN TYPE																										
	ABUTMENT TO ABUTMENT				ABUTMENT TO PIER								PIER TO PIER														
	S1	S2			N1	N2	S1	N1	N2	S2			S3	N3	N4	CP	N5	N6	S2			S3	N3	N4	CP	N5	N6
45'	5'-3"	7 Spa. @ 5'-0" = 35'-0"			6	14	10'-0"	10	25	5 Spa. @ 5'-0" = 25'-0"			5'-3"	6	14	5'-0"	5	13	5 Spa. @ 5'-0" = 25'-0"			5'-0"	5	13	5'-0"	5	13
50'	7'-9"	7 Spa. @ 5'-0" = 35'-0"			8	20	5'-3"	6	14	7 Spa. @ 5'-0" = 35'-0"			5'-0"	5	13	5'-0"	5	13	5 Spa. @ 5'-0" = 25'-0"			7'-6"	8	19	5'-0"	5	13
55'	5'-3"	9 Spa. @ 5'-0" = 45'-0"			6	14	10'-0"	10	25	7 Spa. @ 5'-0" = 35'-0"			5'-3"	6	14	5'-0"	5	13	7 Spa. @ 5'-0" = 35'-0"			5'-0"	5	13	5'-0"	5	13
60'	7'-9"	9 Spa. @ 5'-0" = 45'-0"			8	20	5'-3"	6	14	9 Spa. @ 5'-0" = 45'-0"			5'-0"	5	13	5'-0"	5	13	7 Spa. @ 5'-0" = 35'-0"			7'-6"	8	19	5'-0"	5	13
65'	5'-3"	11 Spa. @ 5'-0" = 55'-0"			6	14	10'-0"	10	25	9 Spa. @ 5'-0" = 45'-0"			5'-3"	6	14	5'-0"	5	13	9 Spa. @ 5'-0" = 45'-0"			5'-0"	5	13	5'-0"	5	13
70'	7'-9"	11 Spa. @ 5'-0" = 55'-0"			8	20	5'-3"	6	14	11 Spa. @ 5'-0" = 55'-0"			5'-0"	5	13	5'-0"	5	13	9 Spa. @ 5'-0" = 45'-0"			7'-6"	8	19	5'-0"	5	13
75'	5'-3"	13 Spa. @ 5'-0" = 65'-0"			6	14	10'-0"	10	25	11 Spa. @ 5'-0" = 55'-0"			5'-3"	6	14	5'-0"	5	13	11 Spa. @ 5'-0" = 55'-0"			5'-0"	5	13	5'-0"	5	13

TYPE III AND TYPE C P.C. BEAMS CONCRETE TRAFFIC RAIL WITH OPENINGS SR1 BAR LIST							
SPAN	EPOXY COATED REINFORCING				SPAN TYPE		
					ABUTMENT TO ABUTMENT	ABUTMENT TO PIER	PIER TO PIER
	MARK	SIZE	FORM	LENGTH	NO.	NO.	NO.
45'	SR1	#5	BNT.	4'-1"	188	218	216
50'	SR1	#5	BNT.	4'-1"	220	220	252
55'	SR1	#5	BNT.	4'-1"	224	254	252
60'	SR1	#5	BNT.	4'-1"	256	256	288
65'	SR1	#5	BNT.	4'-1"	260	290	288
70'	SR1	#5	BNT.	4'-1"	292	292	324
75'	SR1	#5	BNT.	4'-1"	296	326	324

NOTE:  
For bar bend, see Std. TR4-2.

APPROVED BY BRIDGE ENGINEER *David J. Smith* DATE *4/2/10*

OKLAHOMA DEPT. OF TRANSPORTATION  
BRIDGE STANDARD (ENGLISH)  
**CONCRETE TRAFFIC RAIL WITH OPENINGS  
TYPE II, B, III AND C P.C. BEAMS  
INTEGRAL**

2009 SPECIFICATIONS | B40-I-TR4-0-PC23 | O2E  
B-219E