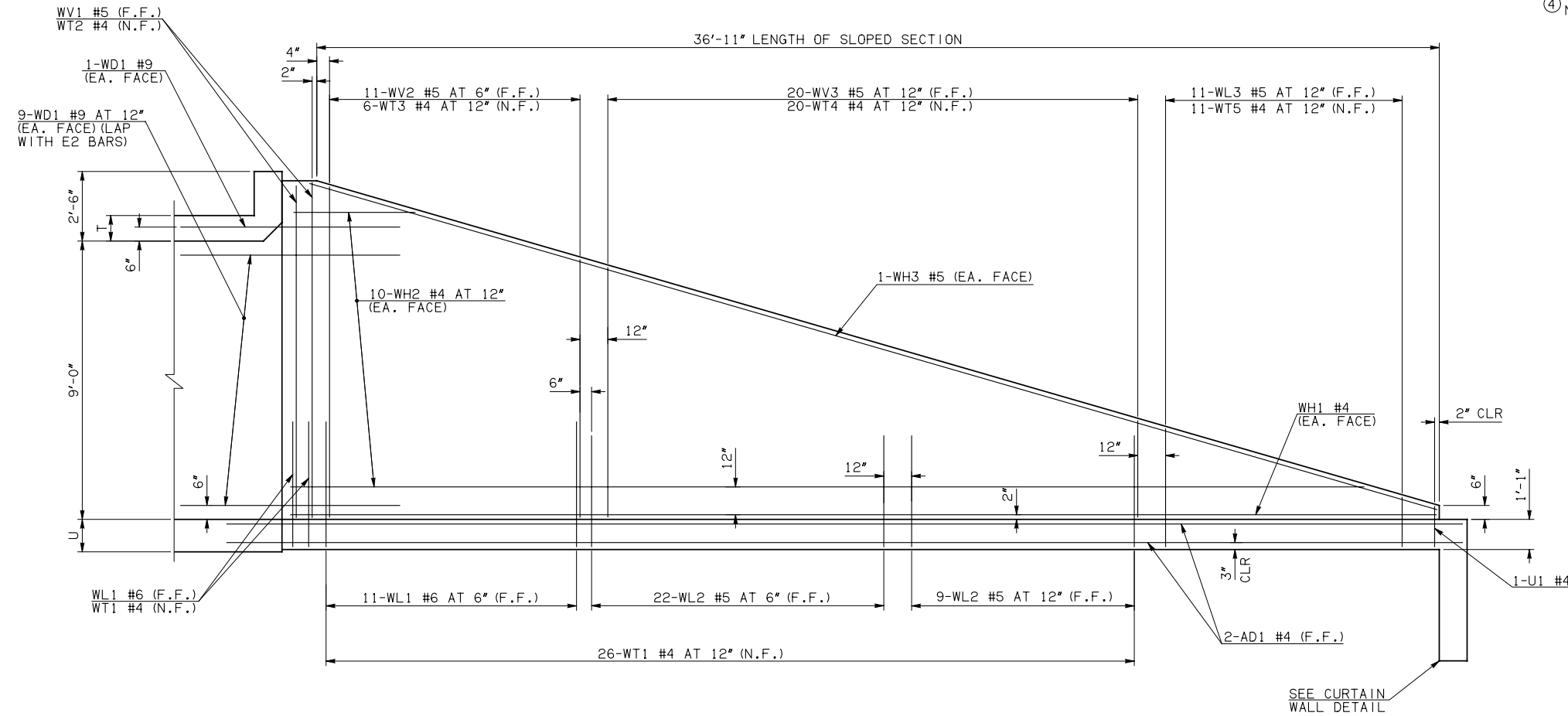
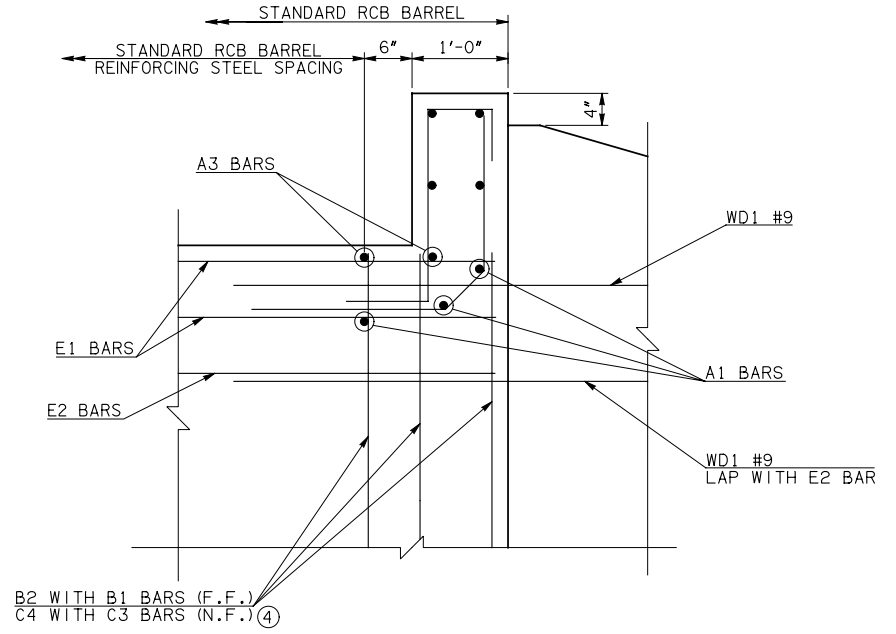


DESCRIPTION	REVISIONS	DATE



④ NOTE: TO CONSTRUCT ONE END SECTION AS SHOWN, THE FOLLOWING BARS ARE REQUIRED IN ADDITION TO THOSE CALLED IN THE RCB BARREL STANDARD:

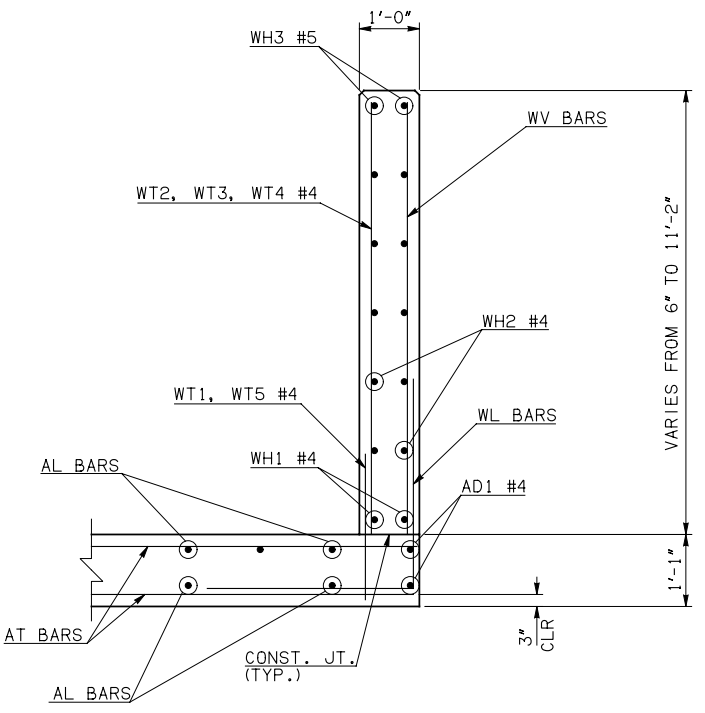
- 2-C3 BARS
  - 2-C4 BARS
- ADDITIONAL WEIGHT IS INCLUDED IN QUANTITIES.



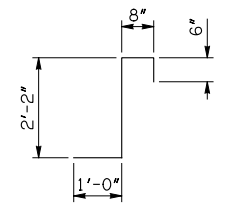
HEADWALL DETAIL AT EXTERIOR WALL

NOTE: F.F. = FAR FACE  
N.F. = NEAR FACE

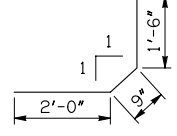
WING ELEVATION



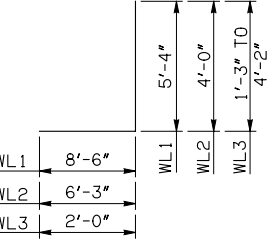
TYPICAL SECTION THRU WING



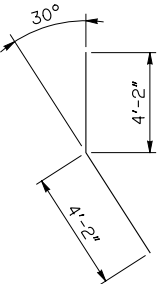
CL1 #4 x 4'-4"



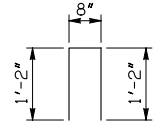
CL2 #4 x 4'-3"



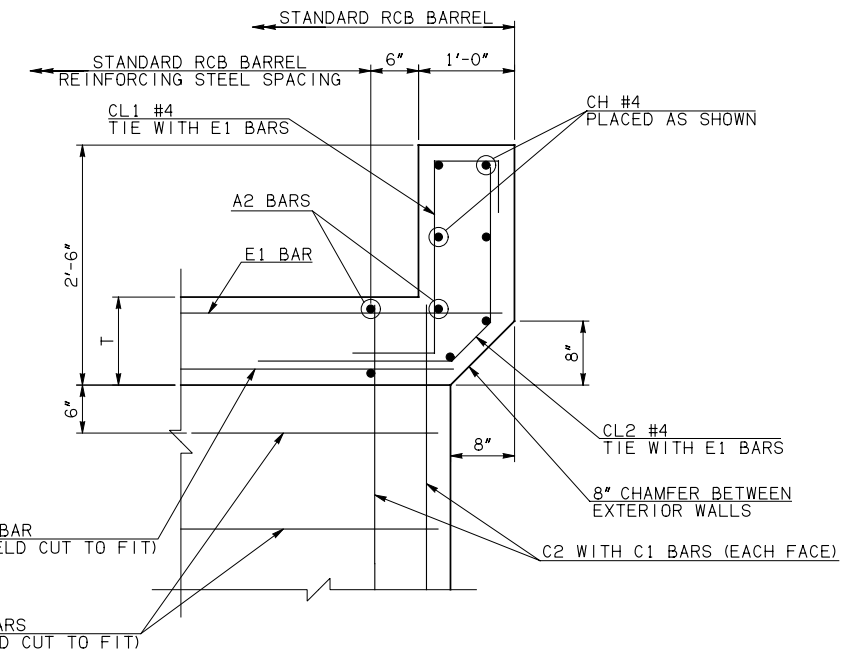
- WL1 #6 x 13'-10"
- WL2 #5 x 10'-3"
- WL3 #5 x 4'-8 1/2" AVG.



WD1 #9 x 8'-4"



U1 #4 x 3'-0"



HEADWALL DETAIL AT INTERIOR WALL

APPROVED BY BRIDGE ENGINEER *Robert J. Nusech* DATE 4/09/09

OKLAHOMA DEPT. OF TRANSPORTATION  
BRIDGE STANDARD (ENGLISH)  
RCB CULVERTS - END SECTION DETAILS  
TRIPLE CELL - 9'-0" HEIGHT - 0°  
SHEET NO. 2 OF 2

1999 SPECIFICATIONS	RCB-E3-H9-0-2	OOE
		B-641E