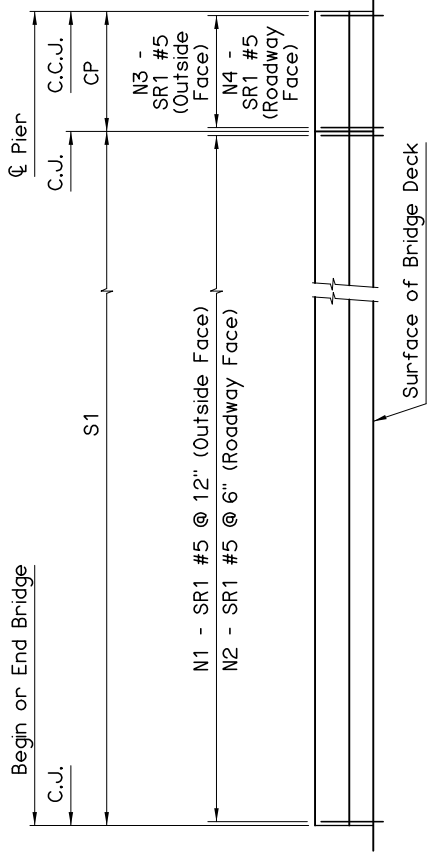
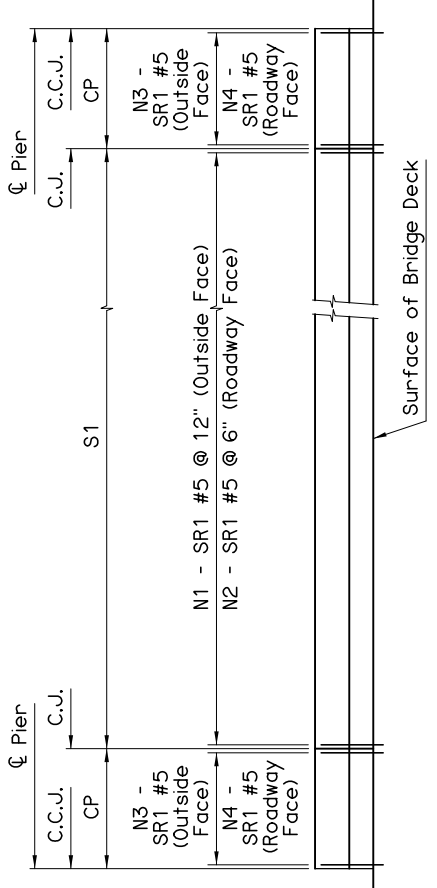


ABUTMENT TO ABUTMENT



ABUTMENT TO PIER



PIER TO PIER

NOTE:
 C.J. indicates a Construction Joint.
 C.C.J. indicates a Control Crack Joint.
 For additional detail of Concrete Traffic Rail, see Std. TR4-1.

CONCRETE TRAFFIC RAIL ELEVATION

TYPE II AND TYPE B P.C. BEAMS
 CONCRETE TRAFFIC RAIL WITHOUT OPENINGS SCHEDULE

SPAN	SPAN TYPE														
	ABUTMENT TO ABUTMENT			ABUTMENT TO PIER						PIER TO PIER					
	S1	N1	N2	S1	N1	N2	CP	N3	N4	S1	N1	N2	CP	N3	N4
30'	30'-6"	32	62	26'-3"	27	53	4'-0"	5	10	22'-0"	23	45	4'-0"	5	10
35'	35'-6"	37	72	31'-3"	32	63	4'-0"	5	10	27'-0"	28	55	4'-0"	5	10
40'	40'-6"	42	82	36'-3"	37	73	4'-0"	5	10	32'-0"	33	65	4'-0"	5	10
45'	45'-6"	47	92	41'-3"	42	83	4'-0"	5	10	37'-0"	38	75	4'-0"	5	10
50'	50'-6"	52	102	46'-3"	47	93	4'-0"	5	10	42'-0"	43	85	4'-0"	5	10

TYPE II AND TYPE B P.C. BEAMS
 CONCRETE TRAFFIC RAIL WITHOUT OPENINGS SR1 BAR LIST

SPAN	EPOXY COATED REINFORCING			SPAN TYPE		
	MARK	SIZE	FORM	ABUTMENT TO ABUTMENT	ABUTMENT TO PIER	PIER TO PIER
30'	SR1	#5	BNT.	4'-1"	188	190
35'	SR1	#5	BNT.	4'-1"	218	220
40'	SR1	#5	BNT.	4'-1"	248	250
45'	SR1	#5	BNT.	4'-1"	278	280
50'	SR1	#5	BNT.	4'-1"	308	310

TYPE III AND TYPE C P.C. BEAMS
 CONCRETE TRAFFIC RAIL WITHOUT OPENINGS SCHEDULE

SPAN	SPAN TYPE														
	ABUTMENT TO ABUTMENT			ABUTMENT TO PIER						PIER TO PIER					
	S1	N1	N2	S1	N1	N2	CP	N3	N4	S1	N1	N2	CP	N3	N4
45'	45'-6"	47	92	40'-3"	41	81	5'-0"	5	13	35'-0"	36	71	5'-0"	5	13
50'	50'-6"	52	102	45'-3"	46	91	5'-0"	5	13	40'-0"	41	81	5'-0"	5	13
55'	55'-6"	57	112	50'-3"	51	101	5'-0"	5	13	45'-0"	46	91	5'-0"	5	13
60'	60'-6"	62	122	55'-3"	56	111	5'-0"	5	13	50'-0"	51	101	5'-0"	5	13
65'	65'-6"	67	132	60'-3"	61	121	5'-0"	5	13	55'-0"	56	111	5'-0"	5	13
70'	70'-6"	72	142	65'-3"	66	131	5'-0"	5	13	60'-0"	61	121	5'-0"	5	13

TYPE III AND TYPE C P.C. BEAMS
 CONCRETE TRAFFIC RAIL WITHOUT OPENINGS SR1 BAR LIST

SPAN	EPOXY COATED REINFORCING			SPAN TYPE		
	MARK	SIZE	FORM	ABUTMENT TO ABUTMENT	ABUTMENT TO PIER	PIER TO PIER
45'	SR1	#5	BNT.	4'-1"	278	280
50'	SR1	#5	BNT.	4'-1"	308	310
55'	SR1	#5	BNT.	4'-1"	338	340
60'	SR1	#5	BNT.	4'-1"	368	370
65'	SR1	#5	BNT.	4'-1"	398	400
70'	SR1	#5	BNT.	4'-1"	428	430

NOTE:
 For bar bend, see Std. TR4-1.