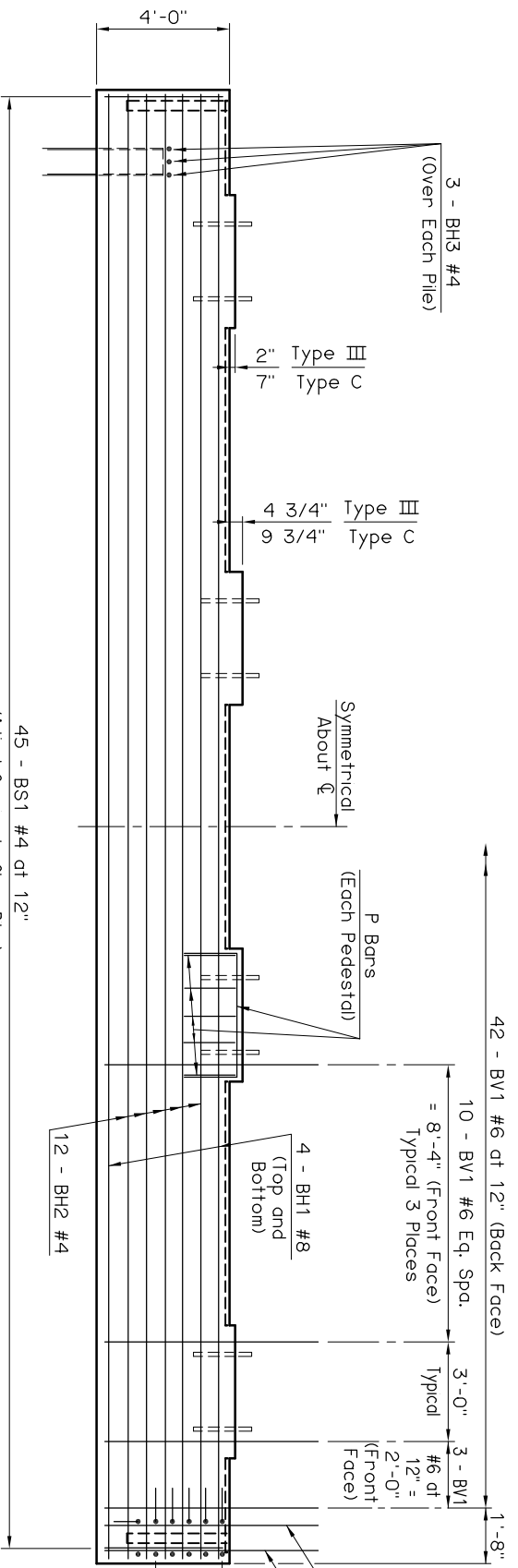
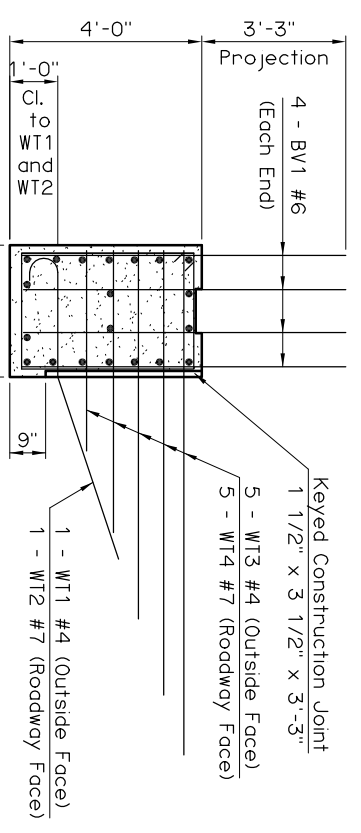


| PILE SCHEDULE |                       |   |       |        |        | MAXIMUM FACTORED PILE LOAD |
|---------------|-----------------------|---|-------|--------|--------|----------------------------|
| SPAN          | TOTAL NUMBER OF PILES | N | A     | B      | C      |                            |
| 45'           | 6                     | 2 | 8'-0" | 4'-0"  | 2'-2"  | 78.4 TON                   |
| 50'           | 7                     | 3 | 6'-9" | 0"     | 1'-11" | 70.4 TON                   |
| 55'           | 7                     | 3 | 6'-9" | 0"     | 1'-11" | 73.4 TON                   |
| 60'           | 7                     | 3 | 6'-9" | 0"     | 1'-11" | 76.4 TON                   |
| 65'           | 8                     | 3 | 5'-8" | 2'-10" | 2'-4"  | 69.4 TON                   |
| 70'           | 8                     | 3 | 5'-8" | 2'-10" | 2'-4"  | 71.9 TON                   |

**PLAN**

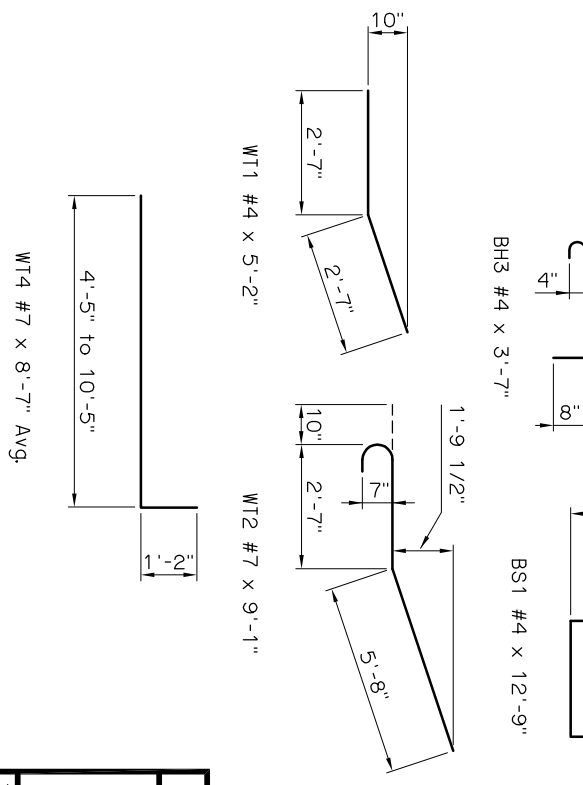
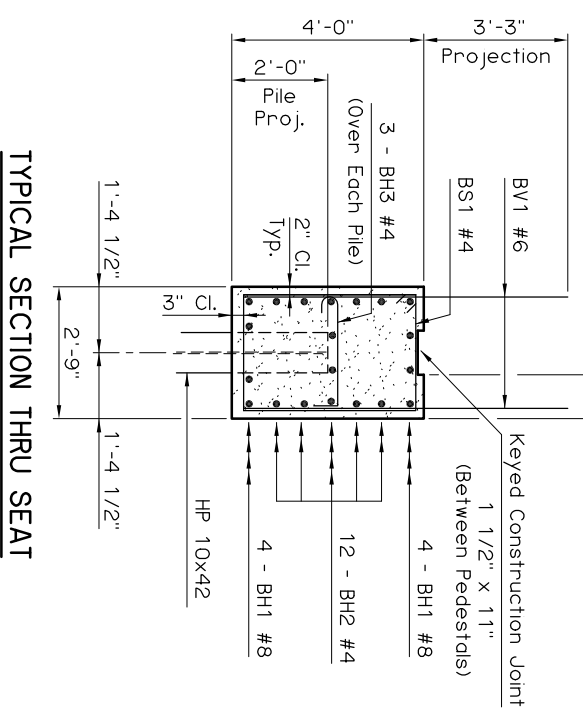


**ELEVATION**



**SECTION A-A**

**TYPICAL SECTION THRU SEAT**



**ABUTMENT SEAT BAR LIST**

| MARK                     | SIZE | NO. | FORM | LENGTH     | LENGTH VARIATION |
|--------------------------|------|-----|------|------------|------------------|
| EPOXY COATED REINFORCING |      |     |      |            |                  |
| BV1                      | #6   | 90  | STR. | 7'-0"      |                  |
| BH1                      | #8   | 8   | STR. | 44'-0"     |                  |
| BH2                      | #4   | 12  | STR. | 44'-0"     |                  |
| BS1                      | #4   | 45  | BNT. | 12'-9"     |                  |
| P1                       | #4   | 20  | BNT. | 6'-3"      |                  |
| P2                       | #4   | 16  | BNT. | 7'-6"      |                  |
| WT1                      | #4   | 2   | BNT. | 5'-2"      |                  |
| WT2                      | #7   | 2   | BNT. | 9'-1"      |                  |
| WT3                      | #4   | 10  | STR. | 7'-5" AVG. | 4'-5" to 10'-5"  |
| WT4                      | #7   | 10  | BNT. | 8'-7" AVG. | 5'-7" to 11'-7"  |
| 6 PILE ABUTMENT          |      |     |      |            |                  |
| 7 PILE ABUTMENT          |      |     |      |            |                  |
| BH3                      | #4   | 21  | BNT. | 3'-7"      |                  |
| 8 PILE ABUTMENT          |      |     |      |            |                  |
| BH3                      | #4   | 24  | BNT. | 3'-7"      |                  |

① 2 Sets of 5

All WT Wing reinforcing tied to Abutment Seat reinforcing must be in place prior to pouring Abutment Seat concrete. Abutment Wings shall not be poured until the Abutment Diaphragm and Deck Slab have attained a strength of 3000 p.s.i. For additional details, see ABUTMENT DIAPHRAGM DETAILS (SHEET 1 OF 2 AND SHEET 2 OF 2) and ABUTMENT WING DETAILS (SHEET 2 OF 2).

APPROVED BY BRIDGE ENGINEER *Chad Head* DATE *8/16/09*

OKLAHOMA DEPT. OF TRANSPORTATION  
BRIDGE STANDARD (ENGLISH)  
ABUTMENT DETAILS  
TYPE III AND TYPE C P.C. BEAMS  
INTEGRAL (SHEET 1 OF 2)

1999 SPECIFICATIONS B40-I-ABUT-PC3-1

ODE B-42E